

STATE OF OHIO  
DEPARTMENT OF TRANSPORTATION  
**D03-BP-FY2006**  
RICHLAND COUNTY  
ASHLAND COUNTY  
WAYNE COUNTY  
MEDINA COUNTY

**PROJECT DESCRIPTION**

WORK ON THIS PROJECT CONSISTS OF VARIOUS WORK ON 26 STRUCTURES IN RICHLAND, ASHLAND, WAYNE AND MEDINA COUNTIES. THIS WORK CONSIST OF SEALING THE CONCRETE ON THE PARAPET, ABUTMENTS AND PIERS, SPOT REPAIRING PARAPETS, PAINTING THE FASCIA BEAMS ON 12 STRUCTURES AND PAINTING THE ENTIRE STRUCTURE AT 7 LOCATIONS. THE SR 89 STRUCTURE WILL HAVE TIMBER SUBDECKING CONSTRUCTED.

PROJECT EARTH DISTURBED AREA - N/A (MAINTENANCE PROJECT)  
ESTIMATED CONTRACTOR EARTH DISTURBED AREA - N/A (MAINTENANCE PROJECT)  
NOTICE OF INTENT EARTH DISTURBED AREA - N/A (MAINTENANCE PROJECT)

**LIMITED ACCESS**

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

**2005 SPECIFICATIONS**

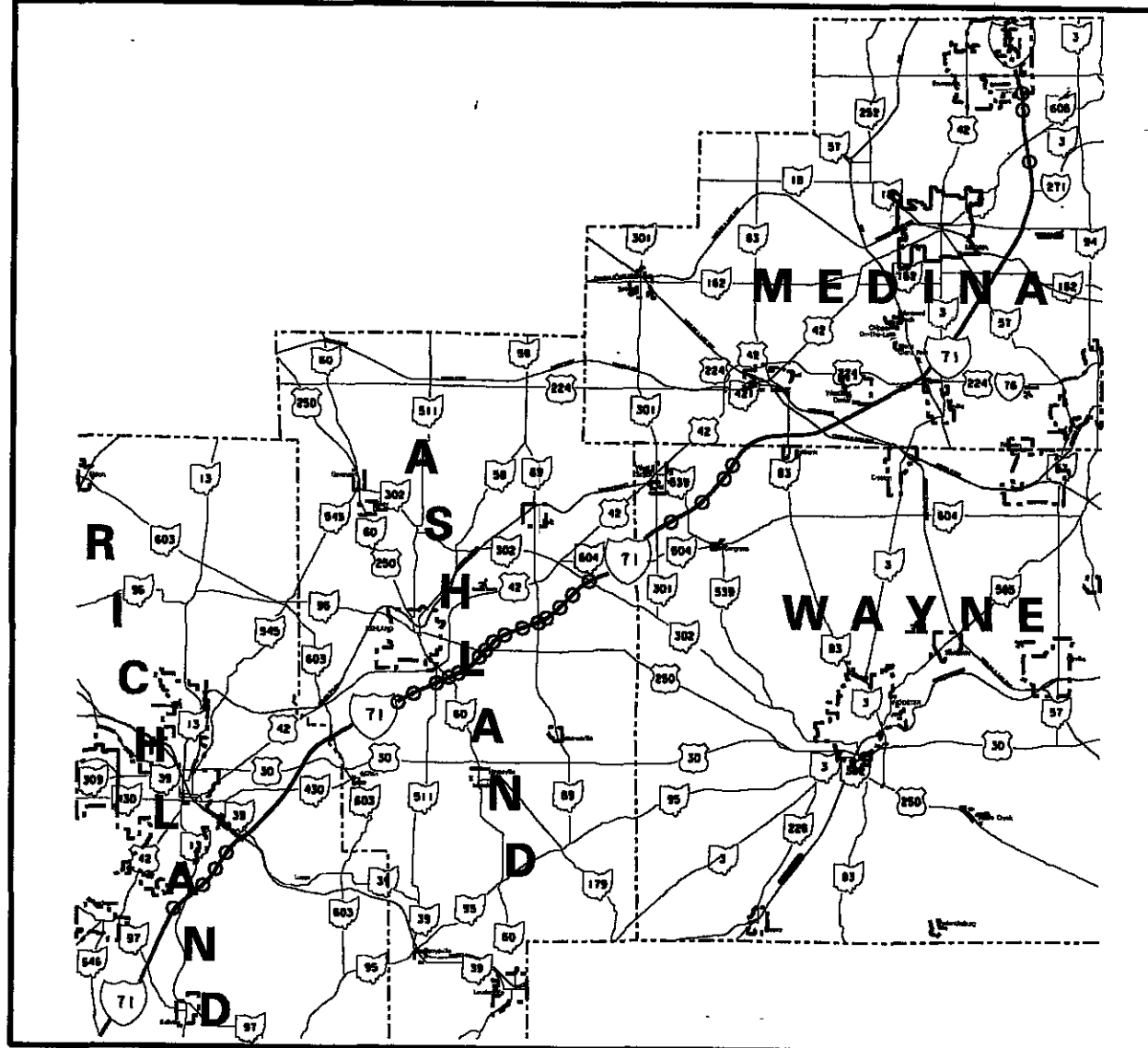
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

**INDEX OF SHEETS:**

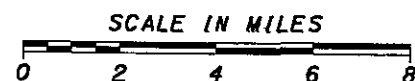
TITLE SHEET  
SCHEMATIC PLAN  
GENERAL NOTES  
MAINTENANCE OF TRAFFIC NOTES  
GENERAL SUMMARY  
STRUCTURE WORK INFORMATION  
COMPLETE STRUCTURE PAINTING DETAILS  
FASCIA BEAM PAINTING DETAILS  
PARAPET REPAIR DETAILS  
PARAPET SEALING DETAILS  
ABUTMENT AND WINGWALL SEALING DETAILS  
PIER SEALING DETAILS  
SUBDECKING DETAILS

1  
2-4  
5, 5A, 5B  
6-8  
9-11  
12  
13  
14  
15-16  
17-18  
19-20  
21  
22

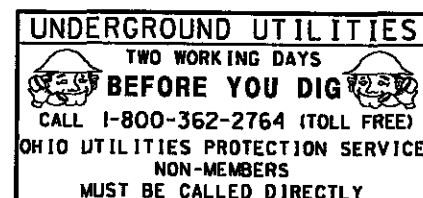


**LOCATION MAP**

LATITUDE: N 40° 56' 08" LONGITUDE: W 82° 06' 33"



PORTIONS TO BE IMPROVED ----- ○  
INTERSTATE & DIVIDED HIGHWAY -----  
UNDIVIDED STATE & FEDERAL ROUTES -----



PLAN PREPARED BY:  
DISTRICT NO. 3  
OHIO DEPARTMENT OF  
TRANSPORTATION  
PRODUCTION DEPARTMENT

ENGINEERS SEAL		STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
	DM-4.3	7/19/02	MT-98.12	4/19/02		800	10/20/06
	DM-4.4	7/19/02	MT-98.13	4/19/02		832	4/17/04
			MT-98.14	4/19/02		833	2/12/03
			MT-98.15	7/16/04			
			MT-98.16	4/19/02			
	MT-35.10	4/20/01	MT-101.20	10/18/02			
	MT-95.30	7/16/04	MT-105.10	10/18/02			
	MT-96.10	4/19/02	MT-105.11	10/18/02			
	MT-96.20	4/19/02					
	MT-96.21	4/19/02	TC-41.10	1/19/01			
	MT-96.25	4/20/01	TC-41.20	1/19/01			
	MT-96.26	4/20/01	TC-52.10	4/20/01			
	MT-97.10	4/19/02	TC-52.20	4/20/01			
SIGNED: David C. Mollenshott							
DATE: 2/10/06							

APPROVED *Thomas M. O'Leary*  
DATE 2-13-06 DISTRICT DEPUTY DIRECTOR

APPROVED *Jordan Proctor*  
DATE 3-15-06 DIRECTOR, DEPARTMENT OF TRANSPORTATION

MATCHLINE

SLM 18.00

US 30 18.78

SLM 19.00

C-92 19.08  
CRIDER RD.

SLM 20.00

SLM 0.00  
RICHLAND CO. 0.00  
SR 603  
ASHLAND CO. 0.00

SLM 1.00

SLM 2.00

ASD-71-0230 (UNDER TR 1153)

SLM 3.00

C-1095 3.13

SLM 4.00

ASD-71-0416 (UNDER CR 995)

SLM 5.00

ASD-71-0527 (UNDER SR 511)

MATCHLINE

SLM 9.00

RIC-71-0978 (UNDER CR 322)

SLM 10.00

SR 13 10.68

SLM 11.00

MANSFIELD CORP

RIC-71-1168 (UNDER TR 308)

SLM 12.00

RIC-71-1264 (UNDER CR 301)

SLM 13.00

RIC-71-1352 (UNDER CR 300)

SLM 14.00

NORFOLK SOUTHERN RR 14.55

SR 39 14.84

SLM 15.00

C-285 15.42  
MT. ZION RD.

SLM 16.00

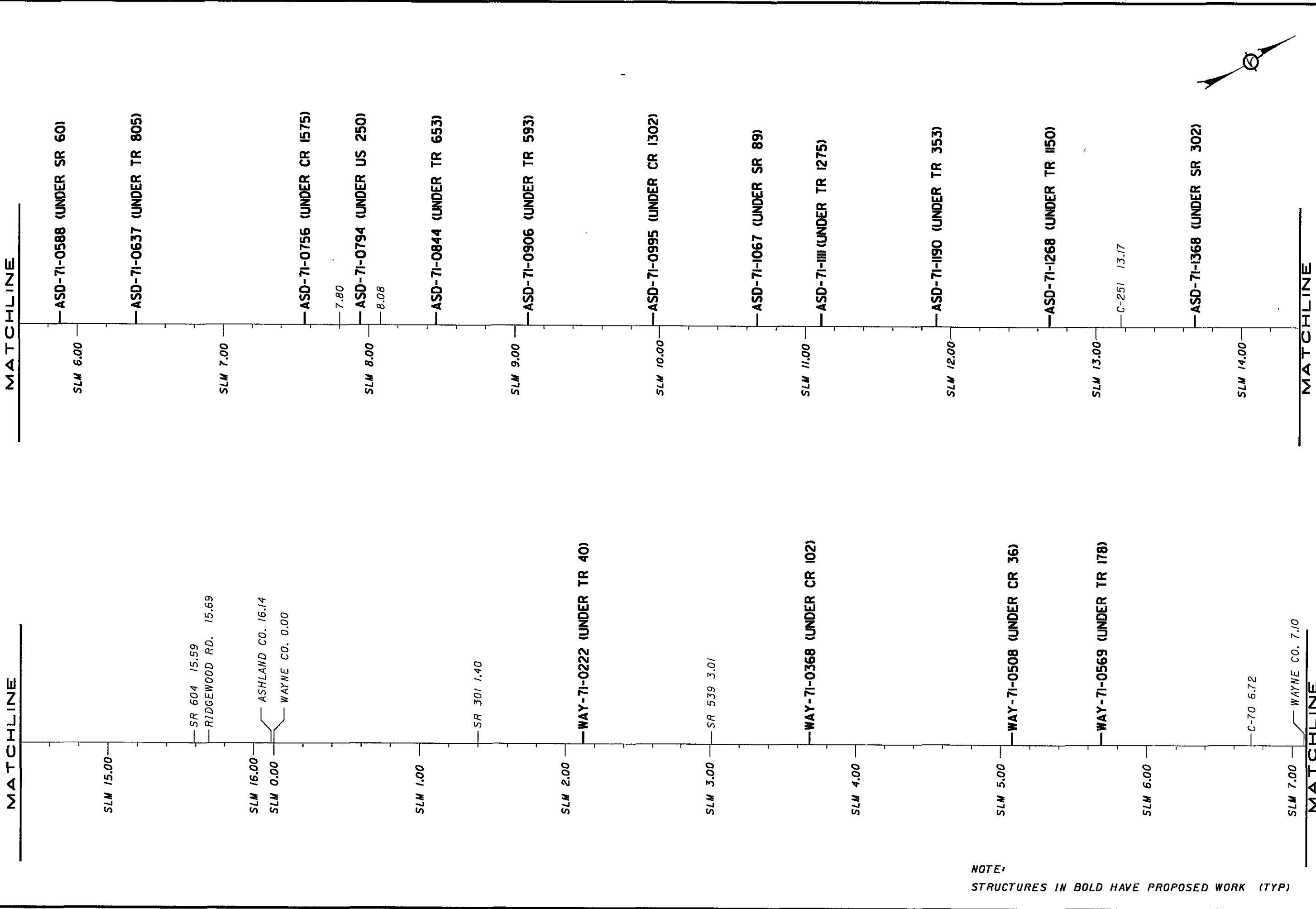
SLM 17.00

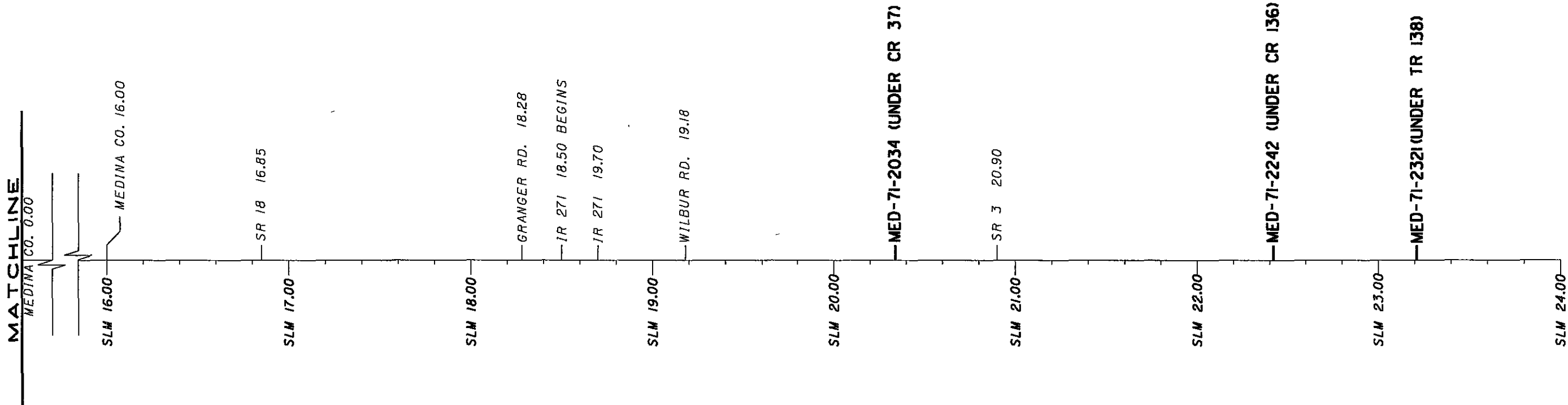
US 430 17.53

MATCHLINE



NOTE:  
STRUCTURES IN BOLD HAVE PROPOSED WORK (TYP)





NOTE:  
STRUCTURES IN BOLD HAVE PROPOSED WORK (TYP)

## EXISTING STRUCTURE VERIFICATION:

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATION AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.02. THE ORIGINAL CONSTRUCTION PLANS OF THE EXISTING BRIDGES ARE AVAILABLE UPON REQUEST AT THE DISTRICT 3 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, ASHLAND, OHIO.

CONTRACT BID PRICES SHALL BE BASED UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PRE-BID EXAMINATION OF THE EXISTING STRUCTURES BY THE CONTRACTOR. HOWEVER, ALL PROJECT WORK SHALL BE BASED ON ACTUAL DETAILS AND DIMENSIONS WHICH HAVE BEEN VERIFIED BY THE CONTRACTOR IN THE FIELD.

## DESIGN SPECIFICATIONS:

THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2002 INCLUDING THE 2003 AND 2004 INTERIM SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL.

## UTILITIES:

THERE ARE NO KNOWN UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

## DESIGN DATA:

CONCRETE CLASS S -COMPRESSION STRENGTH 4500 psi

## CUT LINE CONSTRUCTION JOINT PREPARATION:

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH [25 MM] DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL IN PLACE. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH, BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

## PAINTING STRUCTURAL STEEL - COLORS:

SEE STRUCTURAL WORK INFORMATION ON SHEET 12 FOR FINISH PAINT COLORS.

## PARAPET REPAIR WORK AND SEALING:

THE PARAPET REPAIR WORK SHALL BE COMPLETED A MINIMUM OF FIVE (5) CALENDAR DAYS PRIOR TO SEALING.

## ITEM 202- PORTIONS OF STRUCTURE REMOVED:

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 65-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO CMS 501.05.

## ITEM 511- CLASS S CONCRETE, SUPERSTRUCTURE, AS PER PLAN:

THE COARSE AGGREGATE SHALL BE LIMESTONE.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER CUBIC YARD FOR EACH OF THE ABOVE ITEMS WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

## ITEM 514- SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL, AS PER PLAN:

THIS WORK CONSISTS OF CLEANING THE FASCIA BEAMS AND THE ENTIRE BEARING UNDER THE FASCIA BEAMS USING CMS 514. THE LIMITS OF THE STEEL BEAM SURFACES ARE SHOWN IN THE PLANS.

SURFACE PREPARATION SHALL CONSIST OF CLEANING THE SURFACES TO BE PAINTED AS PER SSPC-SPI, FOLLOWED BY A SURFACE PREPARATION THAT ROUGHENS THE SURFACES TO BE PAINTED WITHOUT CREATING NEGATIVE IMPACT DAMAGE AS PER SSPC-SP7. PAYMENT FOR THE SURFACE PREPARATION SHALL BE PER SQUARE FOOT UNDER ITEM 514- SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL, AS PER PLAN.

THE METHOD OF MEASUREMENT OF THE SURFACE AREA IS DETERMINED BY THE DEPARTMENT AS THE NOMINAL MEASUREMENT OF THE ACTUAL AREAS SPECIFIED TO BE PAINTED.

## ITEM SPECIAL- STRUCTURE, MISC.:TIMBER SUBDECKING:

THIS ITEM SHALL INCLUDE THE SUPPLYING OF MATERIAL AND THE INSTALLATION OF THE TIMBER SUBDECK. ALL WORK SHALL CONFORM TO THE APPROPRIATE SECTIONS OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS.

THE 4 X 4'S CONFORMING TO ITEM 711.26 OF THE CMS SHALL BE GRADE 2 OR BETTER, TREATED IN ACCORDANCE WITH 712.06. THE PLYWOOD SHEETING SHALL BE 3/4" CCA TREATED PLYWOOD.

THE BOLTS SHALL BE 3/8" DIAMETER GALVANIZED BOLTS WITH GALVANIZED FENDER WASHERS AND LOCK NUTS. SPACING OF THE BOLTS SHALL BE A MAXIMUM OF 1'-9".

FIELD MEASUREMENTS SHALL BE TAKEN FOR VERIFICATION BEFORE ANY FABRICATION IS PERFORMED.

THE TIMBER SUBDECK SHALL BE CLEANED OF ANY FALLEN DEBRIS AFTER ALL OTHER WORK ON THE STRUCTURE IS COMPLETED.

THE TIMBER SUBDECK SHALL NOT BE INSTALLED UNTIL AFTER THE STRUCTURAL STEEL IS COMPLETELY PAINTED.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID PER SQUARE FOOT FOR ITEM SPECIAL- STRUCTURE MISC.: TIMBER SUBDECKING WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

# ITEM SPECIAL - PAINTING OF STRUCTURAL STEEL WITH WARRANTY:

**DESCRIPTION.** THIS ITEM CONSISTS OF FURNISHING ALL NECESSARY LABOR, MATERIALS AND EQUIPMENT TO CLEAN, PREPARE AND PAINT ALL STRUCTURAL STEEL SURFACES, AS SPECIFIED IN ITEM 514 OF THE MOST CURRENT VERSION OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, OR AS AMENDED AND ALSO UNCONDITIONALLY WARRANT THE PAINT SYSTEM APPLIED TO THE STRUCTURAL STEEL TO BE FREE OF DEFECTS AS DEFINED IN SECTION WARRANTY ITEMS COVERAGE. ACCEPTANCE BY THE ENGINEER OF ANY PORTION OF THE WORK DURING THE ORIGINAL CLEANING AND PAINTING WILL NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE WARRANTY.

**WARRANTY MAINTENANCE BOND.** PROVIDE THE DEPARTMENT WITH THE PERFORMANCE AND PAYMENT BONDS SPECIFIED IN 103.05, OR AS AMENDED AND FURNISH A THREE (3) YEAR WARRANTY MAINTENANCE BOND EQUAL TO 60 PERCENT OF THE TOTAL PRICE FOR EACH ITEM "SPECIAL PAINTING OF STRUCTURAL STEEL, WITH WARRANTY" AND 60 PERCENT OF THE TOTAL PRICE FOR THE SURFACE PREPARATION AND SHOP APPLICATION OF PRIMER FOR EACH ITEM "513 STRUCTURAL STEEL MEMBERS".

THE SURETY THAT UNDERWRITES THE MAINTENANCE BOND IS REQUIRED TO HAVE AN A.M. BEST RATING OF "A-" OR BETTER. INCLUDE THE COST OF THE MAINTENANCE BOND IN EACH ITEM "SPECIAL PAINTING OF STRUCTURAL STEEL, WITH WARRANTY" AND EACH AFFECTED ITEM "513 STRUCTURAL STEEL MEMBERS". THE EFFECTIVE DATE OF THE MAINTENANCE BOND IS THE DATE THE DEPARTMENT'S FORM C-85 IS ISSUED FOR ALL PAINT ITEMS ON THE PROJECT. AFTER THE C-85 IS ISSUED, THE DEPARTMENT WILL NOTIFY THE SURETY OF THE OFFICIAL START DATE FOR THE WARRANTY BOND AND THE PROJECT WILL BE FINALIZED USING STANDARD PROCEDURES. THE MAINTENANCE BOND EXPIRES AFTER THREE (3) YEARS FROM THE ISSUANCE OF THE C-85 IF NO CORRECTIVE WORK IS REQUIRED OR AFTER COMPLETION OF THE CONTRACTOR'S CORRECTIVE WORK AND APPROVAL BY THE DEPARTMENT.

THE CONTRACTOR MAINTAINS THE LIABILITY INSURANCE SPECIFIED IN 107.12, OR AS AMENDED, COVERING ANY CONTRACTOR OR CONTRACTOR AUTHORIZED OPERATIONS, PERSONS AND EQUIPMENT WHILE ANY CORRECTIVE WORK, OR ANY WARRANTY INSPECTION IS BEING PERFORMED.

**WARRANTY ITEMS COVERAGE.** WARRANT THE ENTIRETY OF THE STRUCTURAL STEEL PAINT SYSTEM AND SURFACE PREPARATION AGAINST THE FOLLOWING DEFECTS FOR THE DURATION OF THE WARRANTY PERIOD:

1. THE OCCURRENCE OF VISIBLE RUST OR RUST BREAKTHROUGH, PAINT BLISTERING, PEELING, SCALING OR UN-REMOVED SLIVERS.
2. PAINT APPLIED OVER DIRT, DEBRIS, BLASTING DEBRIS, OR RUST PRODUCTS NOT REMOVED DURING BLAST CLEANING.
3. MATERIAL DEFICIENCIES, APPLICATION DEFICIENCIES, INCOMPLETE COATINGS, OR COATING THICKNESSES OUTSIDE THE THICKNESS LIMITS SPECIFIED IN THE PAINT SYSTEM SPECIFICATIONS.
4. DAMAGE TO THE COATING SYSTEM CAUSED BY THE CONTRACTOR WHILE REMOVING SCAFFOLDING , FORMS, OR PERFORMING OTHER WORK.

THE DEFECTS WILL INITIALLY BE DETERMINED BY VISUAL INSPECTION. IF THE VISUAL INSPECTION REVEALS A DEFECTIVE CONDITION, DESTRUCTIVE AND NON-DESTRUCTIVE TESTING MAY BE UTILIZED TO DETERMINE THE EXTEND OF THE DEFECT.

MEETING THE MINIMUM REQUIREMENTS AND GUIDELINES OF THE APPLICABLE SPECIFICATION ARE NOT TO BE CONSTRUED AS A WARRANTY, EXPRESSED OR IMPLIED, AS TO THE MATERIALS PROPERTIES AND WORKMANSHIP

EFFORTS REQUIRED TO MEET THE PERFORMANCE CRITERIA. THE INTENT OF THIS CONTRACT IS FOR THE CONTRACTOR TO PROVIDE A MAINTENANCE FREE STRUCTURAL STEEL PAINT SYSTEM. THE CONTRACTOR MAY PERFORM ROUTINE MAINTENANCE DURING THE WARRANTY PERIOD.

EXCLUSIONS TO THE WARRANTY WILL BE DAMAGE TO THE COATING RESULTING FROM VEHICLE DAMAGE, FIRE, OR OTHER DAMAGE NOT CAUSED BY THE CONTRACTOR OR SUBCONTRACTOR.

**TRAFFIC CONTROL.** PROVIDE TRAFFIC CONTROL FOR ALL INSPECTIONS AND CORRECTIVE WORK. THE CONTRACTOR'S TRAFFIC CONTROL PLAN FOR PERFORMING ANY WORK REQUIRED BY THIS SPECIFICATION DURING THE WARRANTY PERIOD SHALL BE IN ACCORDANCE WITH CURRENT DEPARTMENT POLICY AND THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS. SUBMIT THIS TRAFFIC CONTROL PLAN TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) FOR APPROVAL BEFORE ANY WORK IS PERFORMED.

**ANNUAL WARRANTY AND FINAL WARRANTY INSPECTIONS.** THE DEPARTMENT WILL PERFORM TWO (2) ANNUAL INSPECTIONS OF THE WARRANTED ITEMS BEGINNING NO LATER THAN 1 YEAR AFTER THE ISSUANCE OF THE C-85 FOR ALL STRUCTURAL STEEL PAINT SYSTEM AND SURFACE PREPARATION ITEMS ON THE PROJECT. THE DEPARTMENT WILL THEN PERFORM THE FINAL WARRANTY INSPECTION DURING THE MONTH BEFORE THE END OF THE SPECIFIED WARRANTY PERIOD.

FOR EACH ANNUAL WARRANTY INSPECTION AND THE FINAL WARRANTY INSPECTION, THE ENGINEER WILL INSPECT THE BRIDGE THOROUGHLY FOR THE STRUCTURAL STEEL PAINT SYSTEM DEFECT CONDITIONS LISTED. THE INSPECTIONS WILL BE PERFORMED JOINTLY BY ODOT PERSONAL AND THE CONTRACTOR. FURNISH OSHA APPROVED, VEHICLE-MOUNTED INSPECTION EQUIPMENT FOR ALL WARRANTY INSPECTIONS SUITABLE TO PROVIDE ACCESS TO ALL AREAS OF THE STRUCTURE. THE ENGINEER WILL DETERMINE DEFECTIVE AREAS FOR REPAIR AS PER SECTION WARRANTY ITEMS COVERAGE AND DEFINE THOSE AREAS.

**REMEDIAL ACTION.** REPAIR ALL DEFECTIVE AREAS IDENTIFIED AT ANYTIME BY THE ENGINEER DURING THE WARRANTY PERIOD IN ACCORDANCE WITH THE REPAIR PROCEDURES CONTAINED IN ITEM 514 OF THE MOST CURRENT VERSION OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS, OR AS AMENDED. SUBMIT A PROGRESS SCHEDULE AND REMEDIAL ACTION PLAN IN WRITING TO THE ENGINEER PRIOR TO ANY CORRECTIVE WORK. IDENTIFY THE MEANS AND METHODS, MATERIALS AND TIMELINE FOR THE REMEDIAL ACTION. WARRANT THE REMEDIAL ACTION WORK FOR THE REMAINDER OF THE WARRANTY PERIOD. OBTAIN THE ENGINEER'S ACCEPTANCE OF THE PLAN, SCHEDULE AND ALL MATERIALS PRIOR TO PERFORMING REMEDIAL ACTION WORK.

PERFORM ALL STRUCTURAL STEEL PAINT SYSTEM AND SURFACE PREPARATION CORRECTIVE WORK DURING THE SAME SEASON AS THE INSPECTION UNLESS THE SEASONAL LIMITATIONS OF ITEM 514 IN THE MOST CURRENT VERSION OF THE CONSTRUCTION AND MATERIALS SPECIFICATION, AND AS AMENDED, PREVENTS THE COMPLETION THAT SEASON. IF CORRECTIVE WORK CANNOT BE COMPLETED IN THE SAME SEASON DUE TO SEASONAL LIMITATIONS, IMMEDIATELY PERFORM THE CORRECTIVE WORK DURING THE PERMITTED PERIOD IN THE FOLLOWING SEASON. REPAIR ANY ADDITIONAL DEFECTIVE AREAS THAT APPEAR BETWEEN THE TIME OF INSPECTION AND THE ACTUAL PERFORMANCE OF THE CORRECTIVE WORK. NOTIFY THE ENGINEER AT LEAST TWO WEEKS BEFORE BEGINNING THE CORRECTIVE WORK AND ALLOW FULL INSPECTION OF ALL OPERATIONS AS PER ITEM 514 OF THE MOST CURRENT VERSION OF THE CONSTRUCTION AND MATERIALS SPECIFICATION, OR AS AMENDED.

**APPEAL PROCESS.** THE CONTRACTOR MAY APPEAL THE FINDINGS OF THE DEPARTMENT. SUBMIT ANY APPEAL TO THE DCE, IN WRITING, WITHIN FIFTEEN (15) CALENDAR DAYS AFTER THE ENGINEER HAS GIVEN THE WRITTEN

**ITEM SPECIAL - PAINTING OF STRUCTURAL STEEL WITH WARRANTY (CONTINUED):**

RESULTS TO THE CONTRACTOR. THE DCE WILL EVALUATE THE CONTRACTOR'S APPEAL. WITHIN FORTY-FIVE (45) CALENDAR DAYS AFTER RECEIVING THE CONTRACTOR'S APPEAL, THE DCE WILL INFORM THE CONTRACTOR, IN WRITING, OF THE DEPARTMENT'S DETERMINATION.

IF THE CONTRACTOR DISAGREES WITH THE DEPARTMENT'S DETERMINATION, THE CONTRACTOR MAY APPEAL THE DETERMINATION AT STEP 3 OF THE MOST CURRENT VERSION OF PROPOSAL NOTE 109 ENTITLED DISPUTE RESOLUTION AND ADMINISTRATIVE CLAIMS PROCESS, OR AS AMENDED.

**METHOD OF MEASUREMENT.** THE DEPARTMENT WILL MEASURE SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL AND FIELD PAINTING OF EXISTING STRUCTURAL STEEL PRIME COAT BY THE NUMBER OF SQUARE FEET (SQUARE METERS) OF STRUCTURAL STEEL PAINTED OR ON A LUMP SUM BASIS. THE DEPARTMENT WILL MEASURE FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT AND FIELD PAINTING STRUCTURAL STEEL, FINISH COAT BY THE NUMBER OF SQUARE FEET (SQUARE METERS) OR POUNDS (KILOGRAMS) OF STRUCTURAL STEEL PAINTED, OR ON A LUMP SUM BASIS.

THE DEPARTMENT WILL DETERMINE THE NUMBER OF POUNDS (KILOGRAMS) OF NEW STRUCTURAL STEEL PAINTED BY THE ACCEPTED PAY WEIGHT OF THE NEW STRUCTURAL STEEL.

FOR STEEL ROLLED BEAM AND STEEL GIRDER BRIDGES, THE DEPARTMENT WILL DETERMINE THE SURFACE AREA BY TAKING A NOMINAL MEASUREMENT OF THE BEAMS (I.E., TWO TIMES THE BEAM DEPTH PLUS THREE TIMES THE FLANGE WIDTH). IN ADDITION TO THIS NOMINAL MEASUREMENT, THE DEPARTMENT WILL ADD A PERCENTAGE TO ACCOUNT FOR INCIDENTALS SUCH AS CROSS FRAMES, BEARING ASSEMBLIES, STIFFENERS, EXPANSION JOINTS, SCUPPERS, ETC. IT IS NOT NECESSARY FOR THE ENGINEER OR INSPECTOR TO FIELD MEASURE EVERY DETAIL OF THE BRIDGE TO VERIFY QUANTITIES. IF THERE IS A QUANTITY DISPUTE, EXACT FIELD MEASUREMENTS OF ALL PAINTED SURFACES AND CALCULATIONS WILL GOVERN OVER THE ABOVE PERCENTAGE TO ACCOUNT FOR INCIDENTALS.

FOR EXTREMELY COMPLEX BRIDGES, SUCH AS TRUSSES, THE DEPARTMENT WILL PAY FOR PAINTING ON A LUMP SUM BASIS.

THE DEPARTMENT WILL MEASURE GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL BY THE NUMBER OF MAN HOURS EXPENDED BY THE WORKERS ACTUALLY DOING THE GRINDING AND WILL INCLUDE THE TIME WHEN THE WORKERS ARE PERFORMING GRINDING AND REPAIRING PRIME COAT AND NOT LIMITED TO ONLY THE ACTUAL GRINDING DURATION (I.E., THE DEPARTMENT WILL INCLUDE ALL HOURS OF THE WORKERS WHEN ASSIGNED TO GRINDING REGARDLESS OF ACTUAL GRINDING TIME). THE DEPARTMENT WILL NOT MEASURE GRINDING FINS, TEARS, AND SLIVERS ON NEW STEEL BUT WILL CONSIDER IT INCIDENTAL TO UNIT PRICE FOR THE NEW STEEL.

**BASIS OF PAYMENT.** THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICES AS FOLLOWS:

THE DEPARTMENT MAY CONSIDER PAINT AS ELIGIBLE FOR PAYMENT FOR MATERIAL ON-HAND AS SPECIFIED IN 109.10, HOWEVER, ONLY PAINT THAT THE CONTRACTOR CAN PROVE TO THE ENGINEER WILL BE USED DURING THE CONSTRUCTION SEASON IS ELIGIBLE FOR PAYMENT. THE CONTRACTOR WILL PROVIDE THE ENGINEER CALCULATIONS INDICATING THE TOTAL SQUARE FEET (SQUARE METER) OF STEEL TO BE PAINTED DURING THE CONSTRUCTION SEASON. THE CONTRACTOR WILL ALSO PROVIDE CALCULATIONS SHOWING THE TOTAL NUMBER OF GALLONS (LITERS) REQUIRED.

IF THE CONTRACTOR CAUSES DAMAGE OR INJURY TO PUBLIC OR PRIVATE PROPERTY, THE DEPARTMENT WILL NOT PAY FOR RESTORING THE PROPERTY TO ITS ORIGINAL CONDITION.

THE DEPARTMENT WILL NOT PAY FOR REPAIRING ADJACENT COATINGS DAMAGED DURING THE BLASTING OPERATION.

THE DEPARTMENT WILL NOT PAY FOR REMOVING AND REPLACING AN AREA OF COATING BECAUSE A SPOT OR MAXIMUM AVERAGE THICKNESS EXCEEDS THE MAXIMUM SPOT THICKNESS.

THE DEPARTMENT WILL NOT PAY FOR ADDITIONAL TESTING REQUIRED BY ANY HAULER, TREATMENT FACILITY, DISPOSAL FACILITY OR LANDFILL.

THE DEPARTMENT WILL PAY FOR CAULKING UNDER FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT.

THE DEPARTMENT WILL PAY FOR FINAL INSPECTION ACCESS, TEST AREA PREPARATION AND TEST AREA REPAIR AT EACH SELECTED AREA UNDER FINAL INSPECTION REPAIR. THE DEPARTMENT WILL NOT PAY FOR ACCESSING, INSPECTING AND REPAIRING AREAS THAT ARE NOT FOUND TO BE IN CONFORMANCE WITH THE SPECIFICATIONS AND PERTINENT CONTRACT DOCUMENTS.

INCLUDE ALL COSTS ASSOCIATED WITH PROVIDING THE PAINTING OF STRUCTURAL STEEL WITH WARRANTY COMPLETE, IN-PLACE FOR EACH ITEM.

ALL OTHER REQUIREMENTS OF THIS SPECIFICATION ARE CONSIDERED INCIDENTAL TO THE WORK.

ITEM	UNIT	DESCRIPTION
SPECIAL	SQUARE FOOT (SQUARE METER) LUMP SUM	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL, WITH WARRANTY
SPECIAL	SQUARE FOOT (SQUARE METER) LUMP SUM	FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT, WITH WARRANTY
SPECIAL	SQUARE FOOT (SQUARE METER) LUMP SUM POUND (KILOGRAM)	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT, WITH WARRANTY
SPECIAL	SQUARE FOOT (SQUARE METER) LUMP SUM POUND (KILOGRAM)	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT, WITH WARRANTY
SPECIAL	MAN HOUR	GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL
SPECIAL	EACH	FINAL INSPECTION REPAIR

## ITEM 614-MAINTAINING TRAFFIC:

### GENERAL:

LANE RESTRICTIONS ARE ADDRESSED BELOW FOR BOTH THE INTERSTATE AND THE SIDE ROADS. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. MULTIPLE CREWS WILL BE NEEDED TO COMPLETE THE WORK IN THE TIME ALLOTTED PER THE CONTRACT. THE CONTRACTOR SHALL SUBMIT AN ELECTRONIC PROGRESS SCHEDULE EVERY TWO WEEKS TO THE ENGINEER.

INTERIM COMPLETION DATES ARE ADDRESSED IN THE CORRESPONDING NOTE.

### INTERSTATE 71:

TWO (2) LANES OF TRAFFIC (3 LANES REDUCED TO 2 LANES) IN EACH DIRECTION SHALL BE MAINTAINED ON INTERSTATE 71 AT ALL TIMES EXCEPT DURING THOSE TIME PERIODS SHOWN IN THE WORK HOUR RESTRICTIONS NOTES WHERE ONE (1) LANE OF TRAFFIC (3 LANES REDUCED TO 1 LANE) WILL BE PERMITTED IN EACH DIRECTION. FIGURE 6H-37 IN THE OMTCD ADDRESSES A DOUBLE LANE CLOSURE.

RICHLAND COUNTY: WORK IS NOT ALLOWED IN 2006 IN RICHLAND COUNTY DUE TO THE EXISTING CONSTRUCTION OF I-71. THE PROJECT IS EXPECTED TO BE COMPLETED AT THE END OF THE 2006 CONSTRUCTION SEASON.

TRAFFIC ON INTERSTATE 71 SHALL NOT BE PLACED ON THE SHOULDERS. IT IS INTENDED TO UTILIZE THE EXISTING LANES TO MAINTAIN TRAFFIC. THE RUMBLE STRIPS SHALL NOT BE DISTURBED.

### MAXIMUM AND MINIMUM LENGTH OF CLOSURES ON I-71:

THE MAXIMUM LENGTH FOR LANE CLOSURES IS APPROXIMATELY 2 MILES ( $\pm$  0.1 MILE) AND IS TO BE MEASURED AT THE SLM OF THE STRUCTURES AS SHOWN ON THE STRUCTURE WORK INFORMATION SHEET. THE MINIMUM LENGTH BETWEEN CLOSURES SHALL BE 2 MILES ( $\pm$  0.1 MILE) AND IS TO BE MEASURED AT THE SLM OF THE STRUCTURES AS SHOWN ON THE STRUCTURE WORK INFORMATION SHEET.

### ALL OTHER ROADWAYS (EXCLUDING I-71):

ON STRUCTURES WITH TWO TOTAL LANES, A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. ON STRUCTURES WITH MORE THAN TWO TOTAL LANES, A MINIMUM OF ONE LANE IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. DETOURING TRAFFIC IS NOT ALLOWED. THE HOLIDAY WEEKS RESTRICTIONS ON SHEET 7 SHALL ALSO APPLY TO ALL OTHER ROADWAYS.

THE CONTRACTOR MAY USE A SIGNALIZED CLOSURE WITH DRUMS ON THE SIDE ROADS. PAYMENT FOR THE SIGNALIZED CLOSURES SHALL BE INCLUDED IN THE LUMP SUM BID OF ITEM 614 MAINTAINING TRAFFIC. THE CONTRACTOR IS TO NOTIFY THE OWNER OF THE ROAD FIVE (5) CALENDAR DAYS IN ADVANCE OF SETTING UP THE SIGNALIZED CLOSURE.

### LIGHTS ON WORK VEHICLE:

ALL WORK VEHICLES LICENSED TO BE OPERATED ON THE HIGHWAY, INCLUDING MATERIAL TRUCKS, SHALL BE EQUIPPED WITH A FLASHING, ROTATING OR OSCILLATING AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC A MINIMUM OF ONE QUARTER MILE IN BRIGHT SUNLIGHT AND SHALL BE OPERATED WITH LIGHTED HEAD AND TAIL LAMPS. THE AMBER LIGHT SHALL BE IN OPERATION AT ALL TIMES WITHIN THE WORK ZONE AND WHILE TRAVELING TO AND FROM THE WORK ZONE WHENEVER THE VEHICLE SPEED IS BELOW 40 MPH. VEHICLE HAZARD LAMPS DO NOT SATISFY THIS REQUIREMENT. ALL OTHER EQUIPMENT SHALL BE EQUIPPED WITH A FLASHING, ROTATING OR OSCILLATION AMBER LIGHT VISIBLE TO ALL DIRECTIONS OF TRAFFIC A MINIMUM OF ONE QUARTER MILE IN BRIGHT SUNLIGHT. THE AMBER LIGHT SHALL BE IN OPERATION WHILE THE EQUIPMENT IS WITHIN THE WORK ZONE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE UNDER ITEM 614- MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLANS.

### FLOODLIGHTING:

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHT TIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.



**WORK HOUR RESTRICTIONS ON INTERSTATE 71:**

INTERSTATE 71 IS A RESTRICTED WORK CORRIDOR DUE TO THE HIGH TRAFFIC VOLUMES. ALL MAINTENANCE OF TRAFFIC WORK, INCLUDING THE SET UP AND TAKE DOWN OF THE SIGNING AND LANE CLOSURES, WHICH OCCUPIES ANY PORTION OF THE PRESENT TRAVEL LANES IS RESTRICTED TO THE FOLLOWING HOURS:

**NON-HOLIDAY WEEKS (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

SUNDAY 9:00 PM THROUGH MONDAY 6:00 AM  
MONDAY 8:00 PM THROUGH TUESDAY 6:00 AM  
TUESDAY 8:00 PM THROUGH WEDNESDAY 6:00 AM  
WEDNESDAY 8:00 PM THROUGH THURSDAY 6:00 AM  
THURSDAY 8:00 PM THROUGH FRIDAY 6:00 AM

**HOLIDAY WEEKS:**

DURING THE FOLLOWING DESIGNATED HOLIDAYS: MOTHER'S DAY, MEMORIAL DAY, INDEPENDENCE DAY, AND LABOR DAY. NO WORK SHALL BE PERFORMED OVER ANY PORTION OF THE PAVEMENT OR PAVED SHOULDERS AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC ON I.R. 71 EXCEPT AS ADDRESSED IN THE RESTRICTIONS LISTED BELOW. NO WORK WITHIN TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES.

ALL MAINTENANCE OF TRAFFIC WORK, INCLUDING THE SET UP AND TAKE DOWN OF THE SIGNING AND LANE CLOSURES, WHICH OCCUPIES ANY PORTION OF THE PRESENT TRAVEL LANES IS RESTRICTED TO THE FOLLOWING HOURS (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION):

**HOLIDAY FALLS ON A MONDAY (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

TUESDAY 9:00 PM THROUGH WEDNESDAY 6:00 AM  
WEDNESDAY 8:00 PM THROUGH THURSDAY 6:00 AM  
THURSDAY 8:00 PM THROUGH FRIDAY 6:00 AM

**HOLIDAY FALLS ON A TUESDAY (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

SUNDAY 9:00 PM THROUGH MONDAY 6:00 AM  
WEDNESDAY 8:00 PM THROUGH THURSDAY 6:00 AM  
THURSDAY 8:00 PM THROUGH FRIDAY 6:00 AM

**HOLIDAY FALLS ON A WEDNESDAY (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

SUNDAY 9:00 PM THROUGH MONDAY 6:00 AM  
WEDNESDAY 8:00 PM THROUGH THURSDAY 6:00 AM  
THURSDAY 8:00 PM THROUGH FRIDAY 6:00 AM

**HOLIDAY FALLS ON A THURSDAY (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

SUNDAY 9:00 PM THROUGH MONDAY 6:00 AM  
MONDAY 8:00 PM THROUGH TUESDAY 6:00 AM  
TUESDAY 8:00 PM THROUGH WEDNESDAY 6:00 AM

**HOLIDAY FALLS ON A FRIDAY, SATURDAY OR SUNDAY (WHERE ONE (1) LANE OF TRAFFIC WILL BE PERMITTED IN EACH DIRECTION)(3 LANES REDUCED TO 1 LANE):**

SUNDAY 9:00 PM THROUGH MONDAY 6:00 AM  
MONDAY 8:00 PM THROUGH TUESDAY 6:00 AM  
TUESDAY 8:00 PM THROUGH WEDNESDAY 6:00 AM  
WEDNESDAY 8:00 PM THROUGH THURSDAY 6:00 AM

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS ON THIS SHEET, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

## INTERIM COMPLETION DATES AND LIQUIDATED DAMAGES:

THE WORK SPECIFIED IN THE FOLLOWING PARAGRAPHS WAS OBTAINED FROM THE STRUCTURE WORK INFORMATION PLAN SHEET. TO SEE WHICH INTERIM COMPLETION DATE APPLIES TO THE STRUCTURE, MATCH UP THE WORK LISTED BELOW THAT BEST MATCHES WITH THE STRUCTURE.

WHEN WORK AT A STRUCTURE LOCATION SPECIFIES PAINTING THE FASCIA BEAMS AND SEALING THE CONCRETE, THE LANE CLOSURE(S) ON INTERSTATE 71 ARE LIMITED TO A MAXIMUM OF TEN (10) CALENDAR DAYS PER STRUCTURE PER DIRECTION OF TRAVEL. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

WHEN WORK AT A STRUCTURE LOCATION SPECIFIES PAINTING THE ENTIRE STRUCTURE, SEALING THE CONCRETE AND MAY INCLUDE ANY OTHER WORK, THE LANE CLOSURE(S) ON INTERSTATE 71 ARE LIMITED TO A MAXIMUM OF TWENTYONE (21) CALENDAR DAYS PER STRUCTURE PER DIRECTION OF TRAVEL. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

WHEN WORK AT A STRUCTURE LOCATION SPECIFIES SEALING THE CONCRETE AND INCLUDE PARAPET REPAIRS, THE LANE CLOSURE(S) ON INTERSTATE 71 ARE LIMITED TO A MAXIMUM OF TEN (10) CALENDAR DAYS PER STRUCTURE PER DIRECTION OF TRAVEL. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

WHEN WORK AT A STRUCTURE LOCATION SPECIFIES SEALING THE CONCRETE ONLY AND NO OTHER WORK, THE LANE CLOSURE(S) ON INTERSTATE 71 ARE LIMITED TO MAXIMUM OF FIVE (5) CALENDAR DAYS PER STRUCTURE PER DIRECTION OF TRAVEL. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES AS PER 108.07.

## ITEM 614- PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS):

THE INTENT OF THESE SIGNS IS TO NOTIFY TRAFFIC THAT THE EXIT RAMPS ARE OPEN TO TRAFFIC AT THE FOLLOWING LOCATIONS:

SR 13 BOTH EXIT RAMPS IN RICHLAND COUNTY = 2 SIGN MONTHS  
US 250 BOTH EXIT RAMPS IN ASHLAND COUNTY = 4 SIGN MONTHS  
SR 539 BOTH EXIT RAMPS IN WAYNE COUNTY = 2 SIGN MONTHS  
SR 3 NB EXIT RAMP IN MEDINA COUNTY = 1 SIGN MONTH  
SR 303 NB EXIT RAMP IN MEDINA COUNTY = 1 SIGN MONTH

AN ESTIMATED 10 SIGN MONTHS ARE PROVIDED.

## ITEM 614- WORKSITE TRAFFIC SUPERVISOR:

THE CONTRACTOR SHALL EMPLOY (OTHER THAN THE SUPERINTENDENT) AND SUBJECT TO THE APPROVAL OF THE ENGINEER, A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS). THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

1. AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772, CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS).
2. THE NATIONAL SAFETY COUNCIL, TRAFFIC CONTROL ZONES SUPERVISORS COURSE, PHONE NUMBER 1-800-441-5103.
3. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703-235-0528.

THE WTS POSITION IS ESTABLISHED FOR THE PURPOSE OF MONITORING AND CORRECTING ANY TRAFFIC CONTROL DEFICIENCIES IN THE WORK ZONE. THE WTS MUST ALSO COORDINATE WITH ALL LAW ENFORCING AGENCIES RESPONSIBLE FOR THE ROADWAY UNDER CONSTRUCTION AND RETRIEVE ALL CRASH REPORTS (OH-1) THAT OCCUR DURING CONSTRUCTION SEASON. THE WTS SHALL OVERSEE ALL OPERATIONS THAT AFFECT THE MOVEMENT OF VEHICULAR AND PEDESTRIAN TRAFFIC THROUGH THE WORK ZONE. TRAFFIC CONTROL AND CRASH DATA EVALUATION WILL BE THE WTS'S MAIN DUTY WHILE THE WORK ZONE IS IN PLACE.

THE WTS SHALL BE PRESENT WHEN THE WORK ZONE IS BEING SET UP, AND SHALL ALSO BE PRESENT WHEN THE CONTRACTOR OR SUBCONTRACTOR INSTALLS A TRAFFIC RESTRICTION, LANE CLOSURE, ETC. IN LIEU OF THE WTS BEING PRESENT WHEN A SUBCONTRACTOR HAS A WORK ZONE IN PLACE, THE CONTRACTOR MAY USE HIS OWN PERSONNEL THAT IS A CERTIFIED WTS. THE CONTRACTOR OR SUBCONTRACTOR MUST PRESENT A COPY OF HIS WTS CERTIFICATE TO THE PROJECT ENGINEER.

DAILY, INCLUDING WEEKENDS AND HOLIDAYS, THE WTS SHALL SPEND A MINIMUM OF ONE HOUR REVIEWING THE WORK ZONE AND/OR CRASH DATA FOR DEFICIENCIES AND MAINTAINING THE WORK ZONE. THE WTS MUST RECOMMEND SOLUTIONS TO ADDRESS ANY ISSUES THAT ARE POTENTIALLY CREATING CRASHES WITHIN THE WORK ZONE. THE WTS MUST PRESENT THESE RECOMMENDATIONS TO THE ENGINEER AND THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM) FOR APPROVAL AT ALL PROJECT PROGRESS MEETINGS. UPON APPROVAL BY THE ENGINEER AND THE DWZTM, THE CONTRACTOR MUST IMPLEMENT THE RECOMMENDED SOLUTIONS TO THE WORK ZONES WITHIN ONE (1) WEEK. THESE HOURS MAY BE ADJUSTED BY THE ENGINEER BUT MUST BE PERFORMED ONCE A DAY DURING THE CONSTRUCTION SEASONS. THE WTS MUST INSPECT THE WORK ZONE AT THE BEGINNING AND THE END OF EACH WORK DAY AND ONE TIME PER DURING THE HOURS OF DARKNESS.

A RECORD OF EACH DAILY REVIEW SHALL BE GIVEN TO THE PROJECT ENGINEER THE FOLLOWING WORK DAY. ALSO IN WRITING, THE WTS'S REPORTS SHALL INCLUDE: TRAFFIC CONTROL DEVICE CONDITION PLACEMENT, VISIBILITY, TRAFFIC FLOW CONDITIONS, INCIDENTS, ACCIDENTS, CONGESTION POINTS, ADEQUACY OF ADVANCED WARNING SIGNS BEYOND PROJECT LIMITS, INTERACTION OF WORK VEHICLES AND TRAFFIC, PROPER STORAGE OF MATERIALS AND EQUIPMENT.

THE WTS SHALL BE AVAILABLE ON A 24-HOUR BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. A 24-HOUR PHONE NUMBER SHALL BE MADE AVAILABLE TO THE PROJECT ENGINEER IN ORDER TO CONTACT THE WTS. THE WTS SHALL HAVE A PAGER AND THE PHONE NUMBER PROVIDED TO THE PROJECT ENGINEER.

FAILURE OF THE CONTRACTOR TO COMPLY WITH ANY OF THE ABOVE, SHALL CONSTITUTE CAUSE FOR THE PROJECT ENGINEER TO DEDUCT \$500.00 PER DAY FROM MONEY DUE TO THE CONTRACTOR, NOT AS A PENALTY, BUT AS A LIQUIDATED DAMAGE.

THE FOLLOWING ESTIMATED QUANTITY BASED ON THE TIME PERMITTED TO DO THE WORK HAS BEEN INCLUDED FOR THE WORKSITE TRAFFIC SUPERVISOR:

ITEM 614- WORKSITE TRAFFIC SUPERVISOR 10 MONTHS

[illegible]



SHEET NUMBER										PARTICIPATION		ITEM	ITEM EXT.	TOTAL	UNIT	DESCRIPTION	REF. SHEET
8	13	14	15	16	17	18	19	20	21	PROJECT	DISTRICT						
			1.1								1.1	202	11300	1.1	CU YD	PORTIONS OF STRUCTURE REMOVED	
			1.1								1.1	511	34401	1.1	CU YD	CLASS S CONCRETE, SUPERSTRUCTURE, AS PER PLAN	
				410			100			510		512	10100	510	SQ YD	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)	
	14253									14253		SPECIAL	88500050	14253	SQ FT	SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL, WITH WARRANTY	
	14253									14253		SPECIAL	88500056	14253	SQ FT	FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT, WITH WARRANTY	
	14253									14253		SPECIAL	88500060	14253	SQ FT	FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT, WITH WARRANTY	
	14253									14253		SPECIAL	88500066	14253	SQ FT	FIELD PAINTING STRUCTURAL STEEL, FINISH COAT, WITH WARRANTY	
	25									25		SPECIAL	88500504	25	MNHR	GRINDING FINES, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL	
	12									12		SPECIAL	88510000	12	EACH	FINAL INSPECTION REPAIR	

	SFN	COUNTY	ROUTE	SLM	LOCATION UNDER	FINISH PAINT FEDERAL COLOR NUMBER	PROPOSED WORK
1	7004087	RIC	71	9.78	CR 322		SEAL ABUTMENT & PARAPETS
2	7004230	RIC	71	11.68	TR 308	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT, PARAPETS & PIERS
3	7004265	RIC	71	12.64	CR 301	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
4	7004354	RIC	71	13.52	CR 300	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT, PARAPETS & PIERS
5	0302376	ASD	71	2.30	TR 1153		SEAL ABUTMENT & PARAPETS
6	0302465	ASD	71	4.16	CR 995	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT & PARAPETS
7	0305529	ASD	71	5.27	SR 511	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
8	0302228	ASD	71	5.88	SR 60	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
9	0302643	ASD	71	6.37	TR 805	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
10	0302708	ASD	71	7.56	CR 1575	15052 Dark Blue	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT & PARAPETS
11	0304921	ASD	71	7.94	US 250	15052 Dark Blue	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
12	0302880	ASD	71	8.44	TR 653	15052 Dark Blue	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
13	0302910	ASD	71	9.06	TR 593	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
14	0303038	ASD	71	9.95	CR 1302	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT, PARAPETS & PARAPET REPAIRS
15	0303631	ASD	71	10.67	SR 89	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT, PARAPETS, PARAPET REPAIRS & SUBDECKING
16	0303097	ASD	71	11.11	TR 1275	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT & PARAPETS
17	0303127	ASD	71	11.90	TR 353	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT & PARAPETS
18	0303151	ASD	71	12.68	TR 1150	17778 Lt Neutral	PAINT FASCIA BEAMS, SEAL ABUTMENT & PARAPETS
19	0305278	ASD	71	13.68	SR 302	17778 Lt Neutral	PAINT COMPLETE STRUCTURE, SEAL ABUTMENT, PARAPETS & PARAPET REPAIRS
20	8503060	WAY	71	2.22	TR 40		SEAL ABUTMENT, PARAPETS & PIERS
21	8503125	WAY	71	3.68	CR 102		SEAL ABUTMENT, PARAPETS, PIERS & PARAPET REPAIRS
22	8503184	WAY	71	5.08	CR 36		SEAL ABUTMENT, PARAPETS & PIERS
23	8503214	WAY	71	5.69	TR 178		SEAL ABUTMENT, PARAPETS, PIERS & PARAPET REPAIRS
24	5203872	MED	71	20.34	CR 37		SEAL PARAPETS
25	5204054	MED	71	22.42	CR 136	17778 Lt Neutral	PAINT FASCIA BEAMS & SEAL PARAPETS
26	5204119	MED	71	23.21	TR 138	17778 Lt Neutral	PAINT FASCIA BEAMS & SEAL PARAPETS

STRUCTURE INFORMATION				
BRIDGE NO.	SPAN1 (LENGTH)	SPAN2 (LENGTH)	SPAN3 (LENGTH)	SPAN4 (LENGTH)
ASD-71-0416 UNDER C-995	56.75'	92.5'	92.5'	56.75'
	W36X150	W36X230	W36X230	W36X150
ASD-71-0756 UNDER C-1575	73.75'	121.5'	91.5'	55.75'
	GIRDER	GIRDER	GIRDER	GIRDER
ASD-71-0995 UNDER C-1302	57.75'	94.5'	94.5'	57.75'
	W36X150	W36X230	W36X230	W36X150
ASD-71-1067 UNDER SR 89	53.75'	88.5'	88.5'	53.75'
	W36X150	W36X230	W36X230	W36X150
ASD-71-1111 UNDER T-1275	58.75'	96'	96'	58.75'
	W36X150	W36X230	W36X230	W36X150
ASD-71-1190 UNDER T-353	68.75'	113'	113'	68.75'
	GIRDER	GIRDER	GIRDER	GIRDER
ASD-71-1368 UNDER SR 302	57.75'	95.5'	95.5'	57.75'
	W36X160	W36X230	W36X230	W36X160

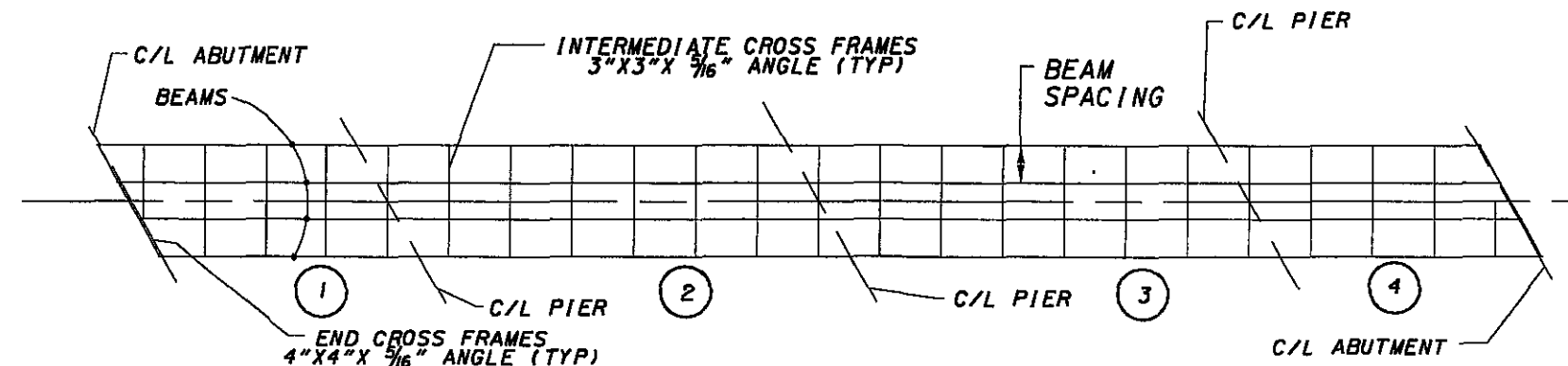
#### SURFACE AREA QUANTITY :

##### FOR ROLLED BEAMS:

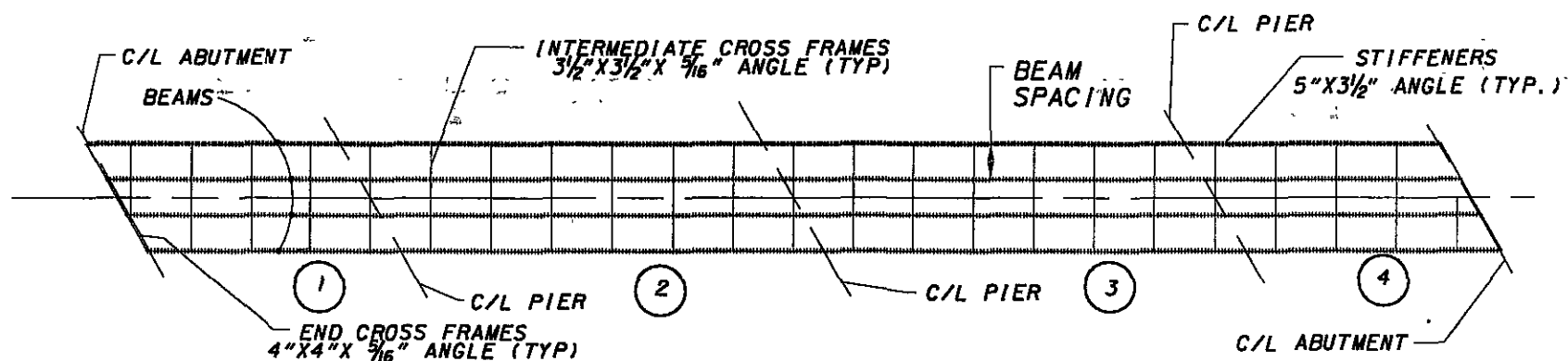
THE DEPARTMENT DETERMINED THE SURFACE AREA BY TAKING A NOMINAL MEASUREMENT OF THE BEAMS ( TWO TIMES THE BEAM DEPTH PLUS THREE TIMES THE FLANGE WIDTH) PLUS ADDED 20% TO ACCOUNT FOR CROSSFRAMES, BEARINGS AND INCIDENTALS.

##### FOR GIRDER BEAMS:

THE DEPARTMENT DETERMINED THE SURFACE AREA BY TAKING A NOMINAL MEASUREMENT OF THE BEAMS ( TWO TIMES THE BEAM DEPTH PLUS THREE TIMES THE FLANGE WIDTH) PLUS ADDED 25% TO ACCOUNT FOR STIFFENERS, CROSSFRAMES, BEARINGS AND INCIDENTALS.



TYPICAL STEEL FRAMING PLAN FOR  
 STRUCTURE ASD-71-0416, ASD-71-0995,  
 ASD-71-1067, ASD-71-1111, ASD-71-1368



TYPICAL STEEL FRAMING PLAN FOR  
 STRUCTURE ASD-71-0756, ASD-71-1190

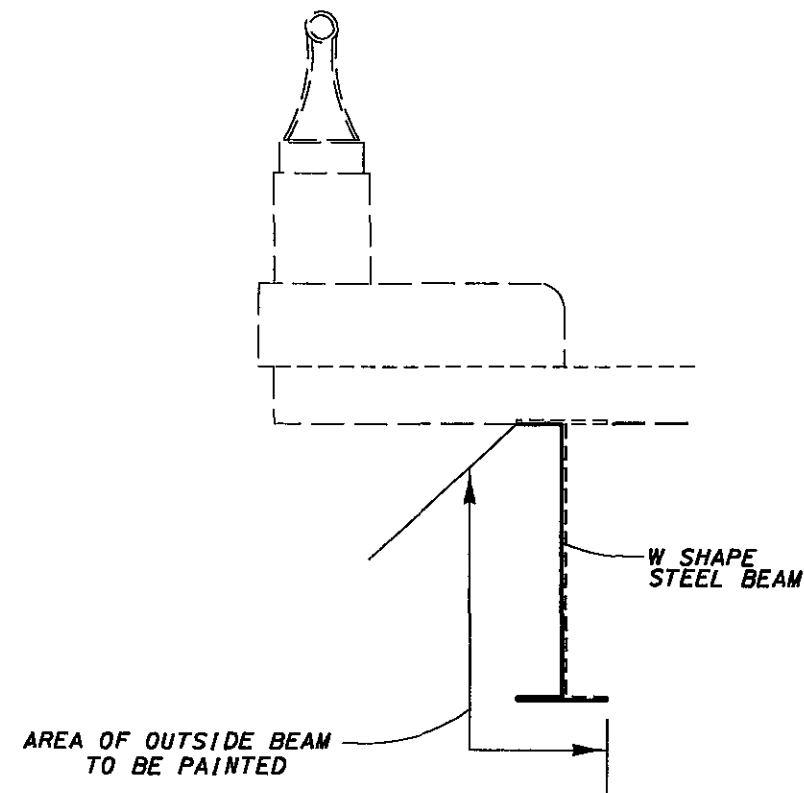
#### LEGEND

① - SPAN NUMBER

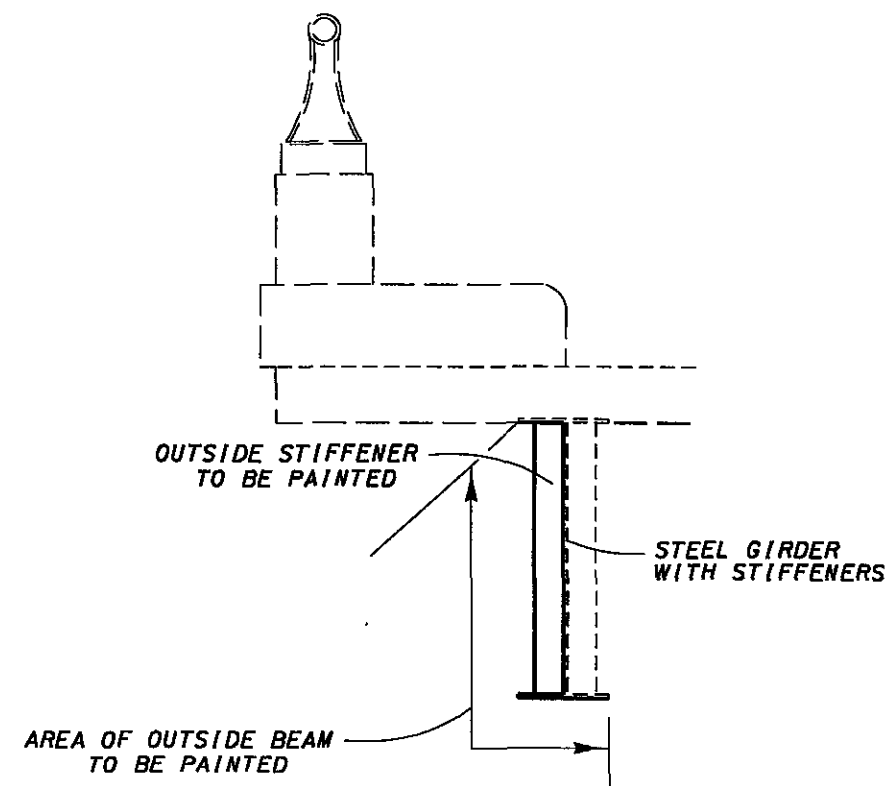
TABLE FOR PAINTING STRUCTURAL STEEL (ALL STEEL SURFACES) (ITEM SPECIAL)

BRIDGE NO.	NO. OF BEAMS TO BE PAINTED	BEAM LENGTH	BEAM HEIGHT	BEAM SPACING	NO. OF SPANS	NO. OF INTERM. CROSS FRAMES	NO. OF END. CROSS FRAMES	* TOTAL SURFACE AREA TO BE PAINTED (SQ. FT.)	* GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL (MANHOUR)	* FINAL INSPECTION REPAIR (EACH)
ASD-71-0416 UNDER C-995	4	298'-6"	36"	8'-0"	4	69	6	13,865	25	11
ASD-71-0756 UNDER C-1575	4	342'-6"	60 1/2"	8'-0"	4	102	6	23,261	25	21
ASD-71-0995 UNDER C-1302	4	304'-6"	36"	8'-0"	4	77	6	14,144	25	12
ASD-71-1067 UNDER SR 89	4	284'-6"	36"	8'-0"	4	69	6	13,217	25	12
ASD-71-1111 UNDER T-1275	4	309'-6"	36"	8'-0"	4	77	6	14,376	25	12
ASD-71-1190 UNDER T-353	4	363'-6"	54 1/2"	8'-0"	4	108	6	22,870	25	22
ASD-71-1368 UNDER SR 302	4	306'-6"	36"	8'-0"	4	68	6	14,253	25	12

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET



TYPICAL STEEL BEAM SECTION  
TO BE PAINTED IF NOT OTHERWISE SPECIFIED



TYPICAL STEEL GIRDER SECTION  
TO BE PAINTED FOR STRUCTURE  
ASD-71-0794, ASD-71-0844, ASD-71-1268

TABLE FOR SURFACE PREPARATION AND FIELD PAINTING STRUCTURAL STEEL (PORTION OF OUTSIDE BEAM ONLY) (ITEM 514)

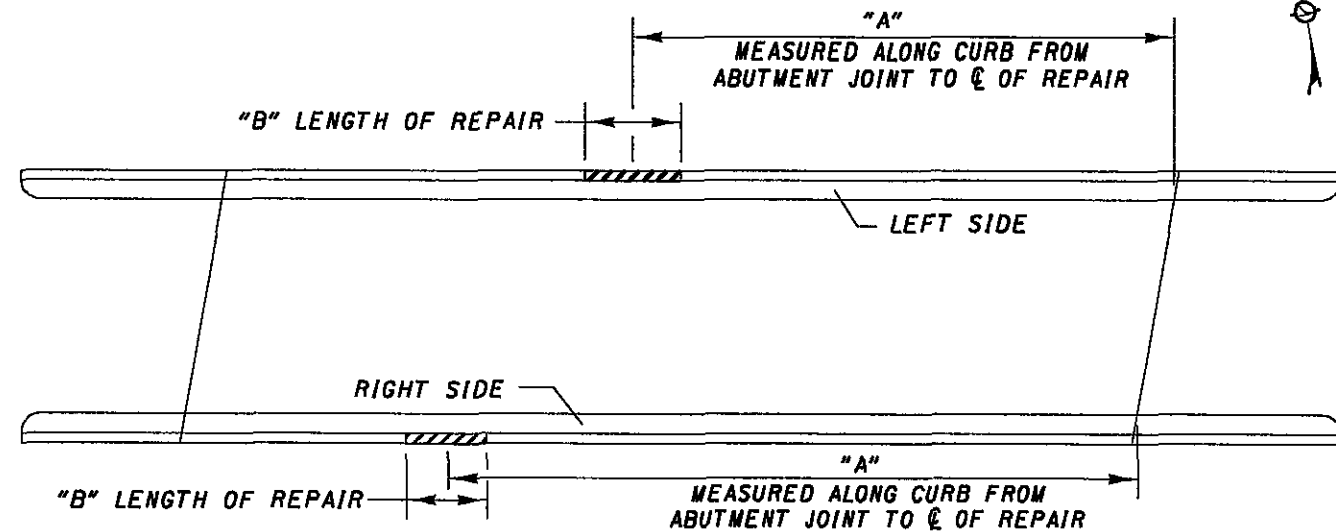
BRIDGE NO.	NO. OF BEAMS TO BE PAINTED	BEAM LENGTH	BEAM SIZE	BEAM TYPE	NO. OF SPANS	NO. OF STIFFENERS	* SURFACE AREA TO BE PAINTED (SQ. FT.)
RIC-71-1168 UNDER T-308	2	289.5'	W36	ROLLED	4		3133
RIC-71-1264 UNDER C-301	2	287'	W36	ROLLED	4		2873
RIC-71-1352 UNDER C-300	2	295.5'	W36	ROLLED	4		3358
ASD-71-0527 UNDER SR 511	2	290.5'	W36	ROLLED	4		3144
ASD-71-0588 UNDER SR 60	2	278'	W36	ROLLED	4		3056
ASD-71-0637 UNDER T-805	2	290'	W36	ROLLED	4		3190
ASD-71-0794 UNDER US 250	2	342'	46 1/2"	GIRDER	4	134	5650
ASD-71-0844 UNDER T-653	2	371'	55"	GIRDER	4	118	5950
ASD-71-0906 UNDER T-593	2	290'	W36	ROLLED	4		2921
ASD-71-1268 UNDER T-1150	2	370'	60"	GIRDER	4	118	6476
MED-71-2242 UNDER C-136	2	278'	W36	ROLLED	4		2806
MED-71-2321 UNDER T-138	2	296.5	W36	ROLLED	4		3243

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET

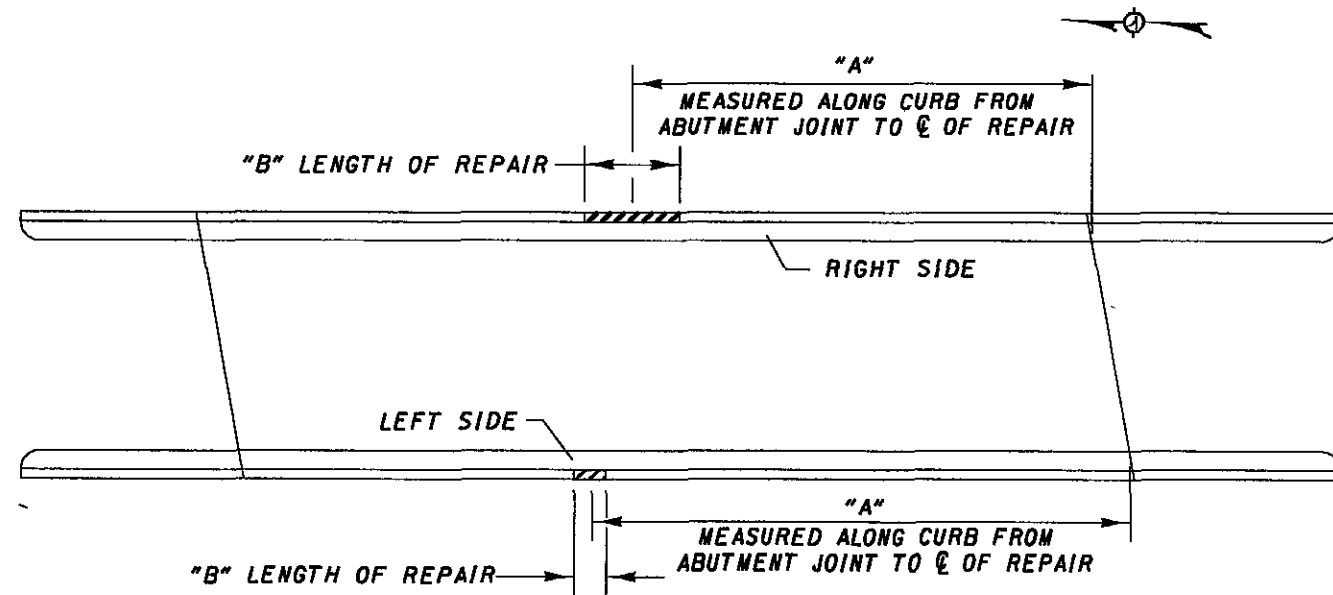
NOTE:

THE QUANTITY OF THE SURFACE AREA INCLUDES THE BEARINGS ON THE OUTSIDE BEAM. THE ENTIRE BEARING INCLUDING THE PLATES ARE TO BE PAINTED UNDER THE OUTSIDE BEAM ONLY.

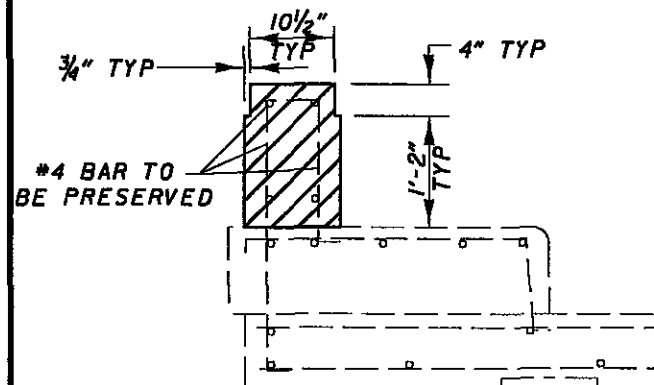




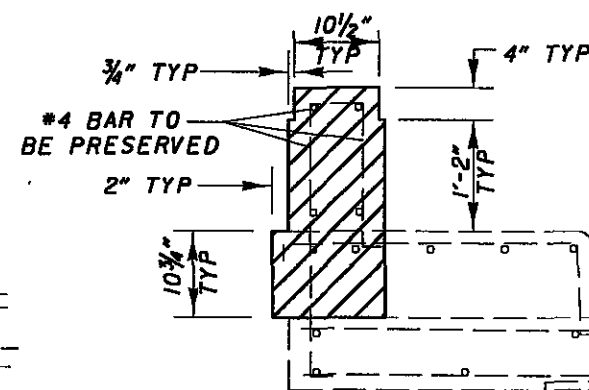
TYPICAL PLAN VIEW FOR STRUCTURE ASD-71-1368, WAY-71-0368, WAY-71-0569



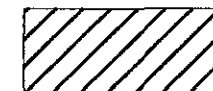
TYPICAL PLAN VIEW FOR STRUCTURE ASD-71-0995, ASD-71-1067



TYPICAL SECTION THROUGH PARAPET REPAIR TYPE 1



TYPICAL SECTION THROUGH PARAPET REPAIR TYPE 2



PORTIONS OF STRUCTURE REMOVED

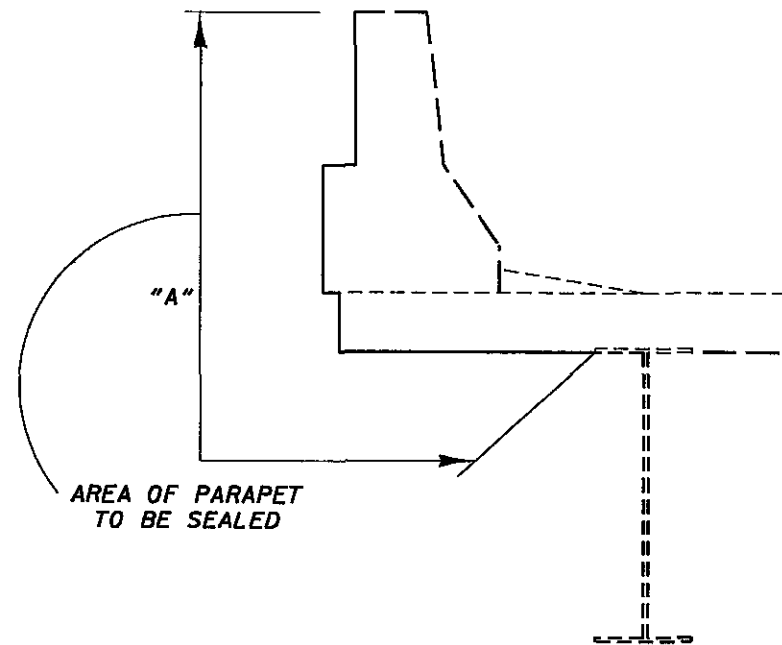
NOTE:

- 1) ALL EXISTING REINFORCING STEEL TO BE PRESERVED
- 2) MATCH EXISTING PARAPET SHAPE WITH PROPOSED PARAPET REPAIR

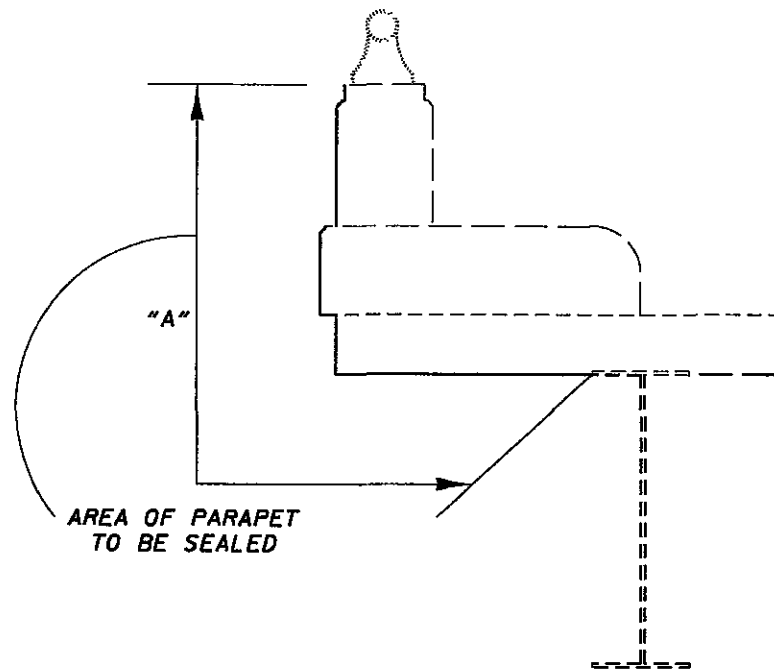
PARAPET REPAIR						
STRUCTURE NO.	SIDE	DISTANCE "A"	DISTANCE "B"	REPAIR TYPE	ITEM 202 PORTION OF STRUCTURE REMOVED (CU. YD.)	ITEM 511 CLASS 5 CONCRETE, AS PER PLAN (CU. YD.)
ASD-71-0995	RIGHT	192'±	1'-6"	1	.1	.1
ASD-71-0995	RIGHT	290'±	1'-6"	1	.1	.1
SUBTOTALS					* .2	* .2
ASD-71-1067	RIGHT	123'±	3'-6"	1	.2	.2
ASD-71-1067	RIGHT	167'±	2'-6"	1	.2	.2
ASD-71-1067	RIGHT	236'±	2'-0"	1	.1	.1
ASD-71-1067	LEFT	32'±	1'-6"	1	.1	.1
ASD-71-1067	LEFT	66'±	2'-0"	1	.1	.1
ASD-71-1067	LEFT	82'±	1'-6"	1	.1	.1
ASD-71-1067	LEFT	100'±	3'-0"	1	.2	.2
ASD-71-1067	LEFT	116'±	1'-6"	1	.1	.1
ASD-71-1067	LEFT	135'-6"±	2'-6"	1	.2	.2
ASD-71-1067	LEFT	170'±	6'-0"	2	.6	.6
ASD-71-1067	LEFT	186'±	7'-0"	2	.7	.7
ASD-71-1067	LEFT	203'±	2'-0"	1	.1	.1
ASD-71-1067	LEFT	220'±	3'-0"	1	.2	.2
SUBTOTALS					* 2.9	* 2.9
ASD-71-1368	LEFT	218'±	2'-6"	1	.2	.2
ASD-71-1368	LEFT	234'-6"±	1'-0"	1	.1	.1
ASD-71-1368	RIGHT	44'±	1'-6"	1	.1	.1
ASD-71-1368	RIGHT	58'±	2'-0"	1	.1	.1
ASD-71-1368	RIGHT	74'±	3'-6"	1	.2	.2
ASD-71-1368	RIGHT	88'±	2'-0"	2	.2	.2
ASD-71-1368	RIGHT	119'±	2'-0"	1	.1	.1
ASD-71-1368	RIGHT	133'±	1'-0"	1	.1	.1
SUBTOTALS					* 1.1	* 1.1

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET





TYPICAL 1



TYPICAL 2

TABLE FOR SEALING OF CONCRETE SURFACES (PARAPET ON BRIDGE)

BRIDGE NO.	LOCATION	TYPICAL	PARAPET LENGTH ON BRIDGE	AVG. DIMENSION "A" (FT.)	SURFACE AREA TO BE SEALED (SQ. YD.)	* TOTAL SURFACE AREA TO BE SEALED (SQ. YD.)
RIC-71-0978 UNDER C-322	LT. SIDE	1	381.9' ±	5.9±	251	502
	RT. SIDE	1	381.9' ±	5.9±	251	
RIC-71-1168 UNDER T-308	LT. SIDE	1	289' ±	6.1±	196	392
	RT. SIDE	1	289' ±	6.1±	196	
RIC-71-1264 UNDER C-301	LT. SIDE	1	286.1' ±	6.2±	197	394
	RT. SIDE	1	286.1' ±	6.2±	197	
RIC-71-1352 UNDER C-300	LT. SIDE	1	295.1' ±	6±	197	394
	RT. SIDE	1	295.1' ±	6±	197	
ASD-71-0230 UNDER T-1153	LT. SIDE	2	294.5' ±	6.2±	203	406
	RT. SIDE	2	294.5' ±	6.2±	203	
ASD-71-0416 UNDER C-995	LT. SIDE	2	299.3' ±	6±	200	400
	RT. SIDE	2	299.3' ±	6±	200	
ASD-71-0527 UNDER SR 511	LT. SIDE	2	291.2' ±	6±	194	388
	RT. SIDE	2	291.2' ±	6±	194	
ASD-71-0588 UNDER SR 60	LT. SIDE	2	279' ±	6±	186	372
	RT. SIDE	2	279' ±	6±	186	
ASD-71-0637 UNDER T-805	LT. SIDE	2	290.7' ±	6±	194	388
	RT. SIDE	2	290.7' ±	6±	194	
ASD-71-0756 UNDER C-1575	LT. SIDE	2	342.1' ±	6±	228	456
	RT. SIDE	2	342.1' ±	6±	228	
ASD-71-0794 UNDER US 250	LT. SIDE	2	338' ±	6.2±	233	466
	RT. SIDE	2	338' ±	6.2±	233	
ASD-71-0844 UNDER T-653	LT. SIDE	2	371.5' ±	6±	248	496
	RT. SIDE	2	371.5' ±	6±	248	
ASD-71-0906 UNDER T-593	LT. SIDE	2	291' ±	6.1±	198	396
	RT. SIDE	2	291' ±	6.1±	198	
ASD-71-0995 UNDER C-1302	LT. SIDE	2	304.8' ±	6±	204	408
	RT. SIDE	2	304.8' ±	6±	204	
ASD-71-1067 UNDER SR 89	LT. SIDE	2	284.7' ±	6±	190	380
	RT. SIDE	2	284.7' ±	6±	190	
ASD-71-1111 UNDER T-1275	LT. SIDE	2	310.6' ±	6±	207	414
	RT. SIDE	2	310.6' ±	6±	207	
ASD-71-1190 UNDER T-353	LT. SIDE	2	366.8' ±	6±	245	490
	RT. SIDE	2	366.8' ±	6±	245	
ASD-71-1268 UNDER T-1150	LT. SIDE	2	370.5' ±	6±	247	494
	RT. SIDE	2	370.5' ±	6±	247	
ASD-71-1368 UNDER SR 302	LT. SIDE	2	307' ±	6±	205	410
	RT. SIDE	2	307' ±	6±	205	

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET

DESIGN AGENCY  
DISTRICT THREE  
OFFICE OF PRODUCTION

DATE  
1/06

REVIEWED  
CAL

DRAWN  
DCM

DESIGNED  
DCM

CHECKED  
DJV

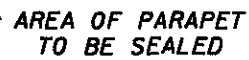
SEALING OF CONCRETE PARAPET  
SURFACES FOR STRUCTURES

D03-BP-FY2006

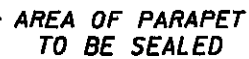
1/2

17  
22

DESIGN FILE: i:\projects\1895\Struct\secdetail.dgn



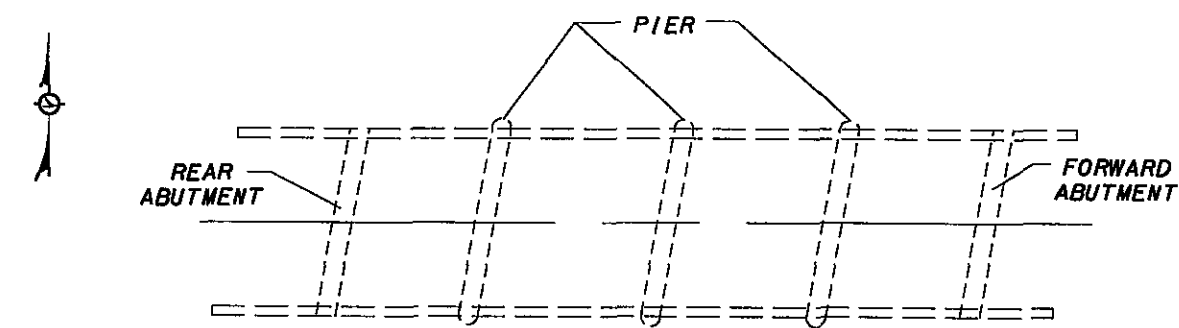
TYPICAL 1



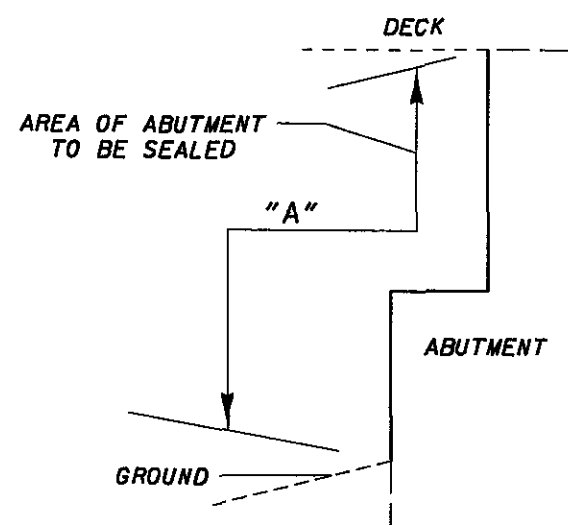
TYPICAL 2

TABLE FOR SEALING OF CONCRETE SURFACES (PARAPET ON BRIDGE)

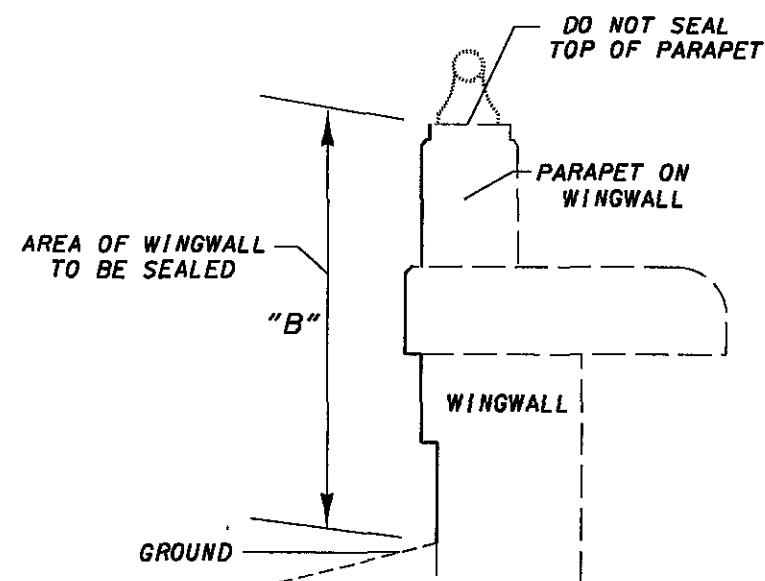
\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET



TYPICAL PLAN VIEW



TYPICAL ABUTMENT ELEVATION VIEW



TYPICAL WINGWALL ELEVATION VIEW

NOTES:

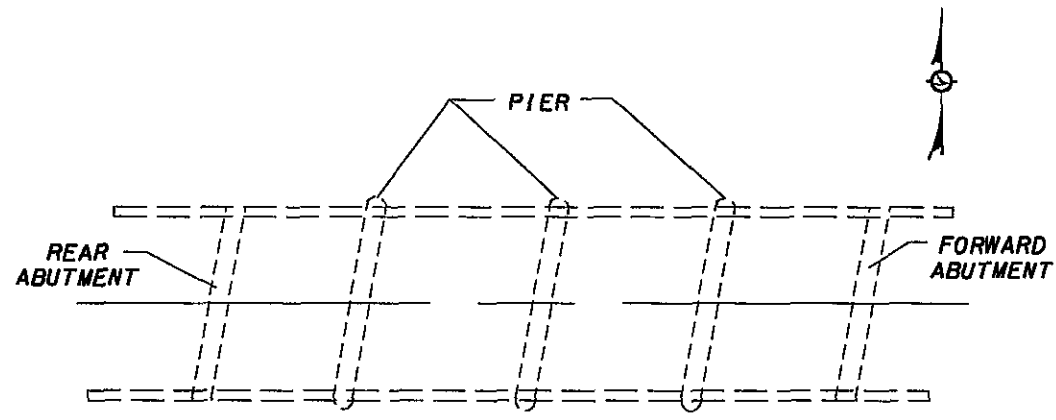
SEAL ALL EXPOSED ABUTMENT CONCRETE AND OUTSIDE EDGE OF PARAPET AND WINGWALL.

TABLE FOR SEALING OF CONCRETE SURFACES (ABUTMENT AND WINGWALLS)

BRIDGE NO.	LOCATION	AVG. DIMENSION "A"	ABUTMENT LENGTH	AVG. DIMENSION "B"	AVERAGE WINGWALL LENGTH	SURFACE AREA TO BE SEALED (SQ. YD.)	* TOTAL SURFACE AREA TO BE SEALED (SQ. YD.)
RIC-71-978 UNDER C-322	FORWARD ABUT.	11'-2"±	37'-3"±	8'-6"±	23' ±	90	177
	REAR ABUT.	11'-2"±	38'-6"±	8'-6"±	20'-6"±	87	
RIC-71-1168 UNDER T-308	FORWARD ABUT.	9'-10"±	35' ±	8' ±	14'-8"±	65	169
	REAR ABUT.	13'-8"±	34' ±	10' ±	23'-6"±	104	
RIC-71-1264 UNDER C-301	FORWARD ABUT.	9'-10"±	31'-10"±	8' ±	14'-5"±	61	124
	REAR ABUT.	10'-4"±	31'-10"±	8'-2"±	14'-3"±	63	
RIC-71-1352 UNDER C-300	FORWARD ABUT.	12'-4"±	36' ±	9' ±	21'-3"±	92	154
	REAR ABUT.	9'-4"±	36' ±	7'-8"±	14' ±	62	
ASD-71-0230 UNDER T-1153	FORWARD ABUT.	10' ±	33'-6"±	7'-4"±	14'-6"±	61	122
	REAR ABUT.	10' ±	33'-6"±	7'-4"±	14'-6"±	61	
ASD-71-0416 UNDER C-995	FORWARD ABUT.	8'-6"±	34' ±	7' ±	14'-1"±	54	113
	REAR ABUT.	9'-6"±	34' ±	7'-4"±	14'-1"±	59	
ASD-71-0527 UNDER SR 511	FORWARD ABUT.	9' ±	33' ±	7' ±	14' ±	55	110
	REAR ABUT.	9' ±	33' ±	7' ±	14' ±	55	
ASD-71-0588 UNDER SR 60	FORWARD ABUT.	8'-4"±	38' ±	6'-9"±	14'-11"±	58	122
	REAR ABUT.	9'-4"±	38' ±	7'-3"±	14'-11"±	64	
ASD-71-0637 UNDER T-805	FORWARD ABUT.	8'-6"±	32'-6"±	7' ±	13'-9"±	53	106
	REAR ABUT.	8'-6"±	32'-6"±	7' ±	13'-9"±	53	
ASD-71-0756 UNDER C-1575	FORWARD ABUT.	10'-8"±	32'-6"±	8' ±	19'-4"±	73	146
	REAR ABUT.	10'-8"±	32'-6"±	8' ±	19'-4"±	73	
ASD-71-0794 UNDER US 250	FORWARD ABUT.	9' ±	102' ±	7'-7"±	17'-9"±	132	272
	REAR ABUT.	9'-6"±	102' ±	8' ±	17'-9"±	140	
ASD-71-0844 UNDER T-653	FORWARD ABUT.	9'-6"±	41' ±	7'-4"±	19'-3"±	75	150
	REAR ABUT.	9'-6"±	41' ±	7'-4"±	19'-3"±	75	
ASD-71-0906 UNDER T-593	FORWARD ABUT.	7'-4"±	33' ±	6'-3"±	13'-7"±	46	94
	REAR ABUT.	7'-9"±	33' ±	6'-6"±	13'-7"±	48	
ASD-71-0995 UNDER C-1302	FORWARD ABUT.	8' ±	34'-6"±	6'-6"±	14'-3"±	52	106
	REAR ABUT.	8'-6"±	34'-6"±	6'-9"±	14'-3"±	54	
ASD-71-1067 UNDER SR 89	FORWARD ABUT.	8' ±	32'-6"±	6'-6"±	13'-6"±	49	100
	REAR ABUT.	8'-6"±	32'-6"±	6'-9"±	13'-6"±	51	
ASD-71-1111 UNDER T-1275	FORWARD ABUT.	8'-6"±	35' ±	6'-9"±	14'-2"±	55	110
	REAR ABUT.	8'-6"±	35' ±	6'-9"±	14'-2"±	55	
ASD-71-1190 UNDER T-353	FORWARD ABUT.	9'-6"±	43' ±	7'-3"±	19'-6"±	77	158
	REAR ABUT.	10' ±	43' ±	7'-6"±	19'-6"±	86	
ASD-71-1268 UNDER T-1150	FORWARD ABUT.	10' ±	41'-6"±	7'-7"±	22' ±	84	172
	REAR ABUT.	10'-6"±	41'-6"±	8' ±	22' ±	88	
ASD-71-1368 UNDER SR 302	FORWARD ABUT.	8' ±	34' ±	6'-8"±	13' ±	50	100
	REAR ABUT.	8' ±	34' ±	6'-8"±	13' ±	50	

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET

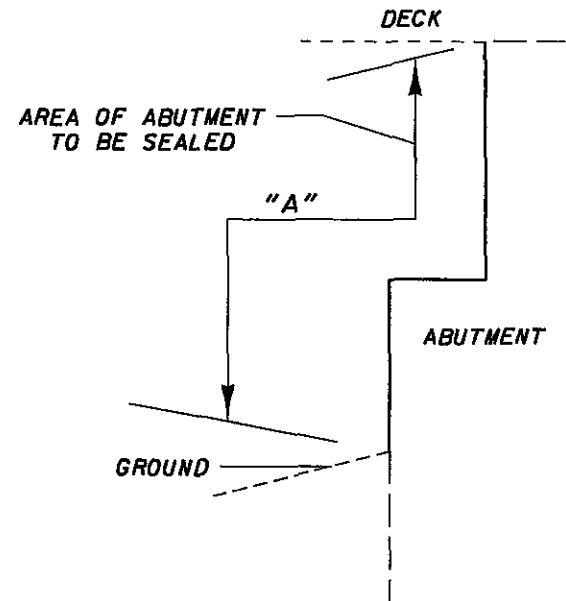
DESIGN FILE: i:\projects\18951\Struct\seal\detail.dgn  
WORKSTATION: dml/jens DATE: 2/13/2006



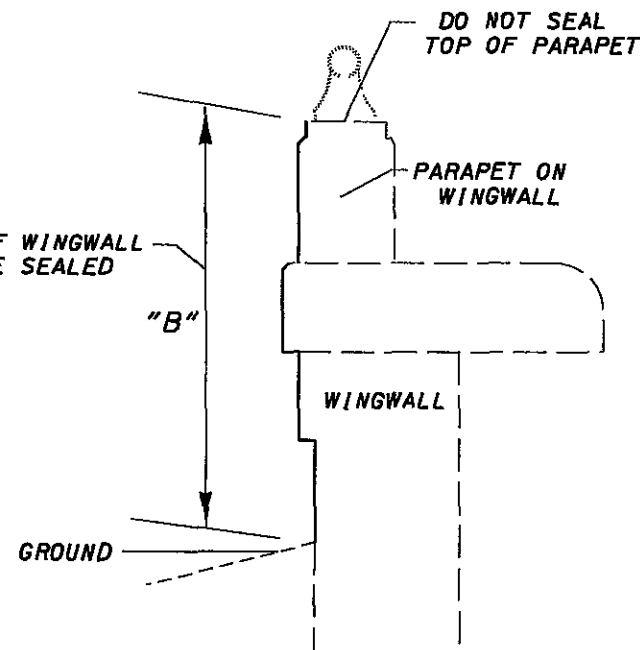
TYPICAL PLAN VIEW

TABLE FOR SEALING OF CONCRETE SURFACES (ABUTMENT AND WINGWALLS)							
BRIDGE NO.	LOCATION	AVG. DIMENSION "A"	ABUTMENT LENGTH	AVG. DIMENSION "B"	AVERAGE WINGWALL LENGTH	SURFACE AREA TO BE SEALED (SQ. YD.)	* TOTAL SURFACE AREA TO BE SEALED (SQ. YD.)
WAY-71-0222 UNDER T-40	FORWARD ABUT.	8' ±	30' - 3" ±	6' - 8" ±	13' - 2" ±	47	94
	REAR ABUT.	8' ±	30' - 3" ±	6' - 8" ±	13' - 2" ±	47	
WAY-71-0368 UNDER C-102	FORWARD ABUT.	10' - 3" ±	45' ±	7' - 8" ±	22' - 6" ±	90	180
	REAR ABUT.	10' - 3" ±	45' ±	7' - 8" ±	22' - 6" ±	90	
WAY-71-0508 UNDER C-36	FORWARD ABUT.	10' - 3" ±	46' ±	7' - 8" ±	22' - 3" ±	91	182
	REAR ABUT.	10' - 3" ±	46' ±	7' - 8" ±	22' - 3" ±	91	
WAY-71-0569 UNDER T-178	FORWARD ABUT.	9' ±	36' ±	7' - 3" ±	16' - 7" ±	63	132
	REAR ABUT.	9' ±	36' ±	7' - 3" ±	20' - 2" ±	69	

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET



TYPICAL ABUTMENT ELEVATION VIEW



TYPICAL WINGWALL ELEVATION VIEW

NOTES:

SEAL ALL EXPOSED ABUTMENT CONCRETE AND OUTSIDE EDGE OF PARAPET AND WINGWALL.

DESIGN AGENT  
DISTRICT THREE  
OFFICE OF PRODUCTION

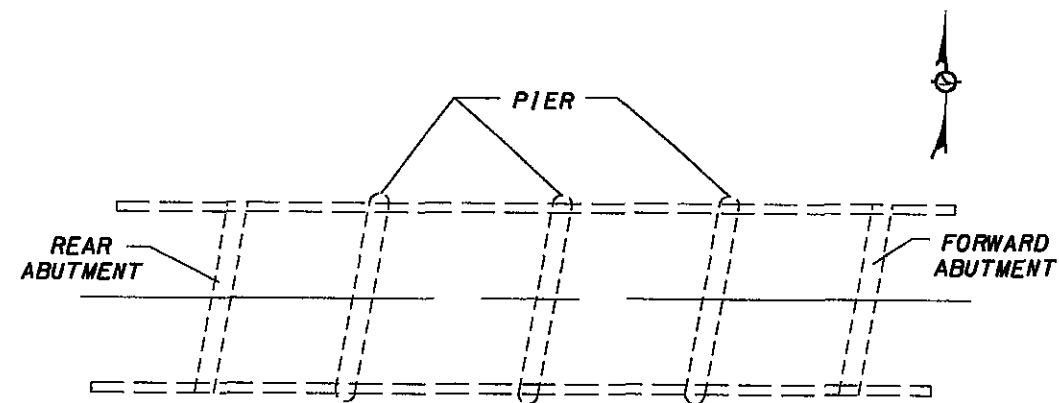
DATE  
1/06  
REVIEWED  
CAL  
DRAWN  
DCM  
CHECKED  
DJV

SEALING OF CONCRETE ABUTMENT AND WINGWALL  
SURFACES FOR STRUCTURES

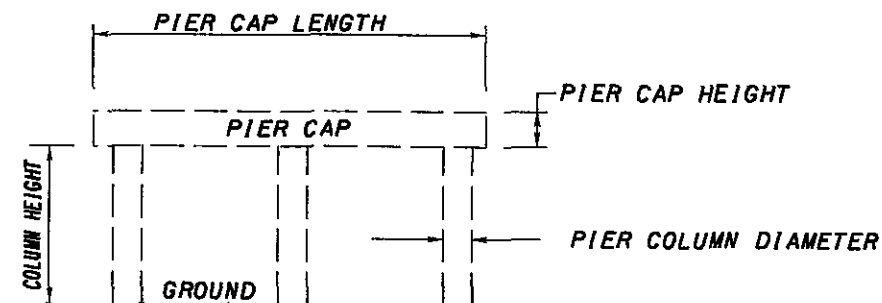
D03-BP-FY2006

2/2

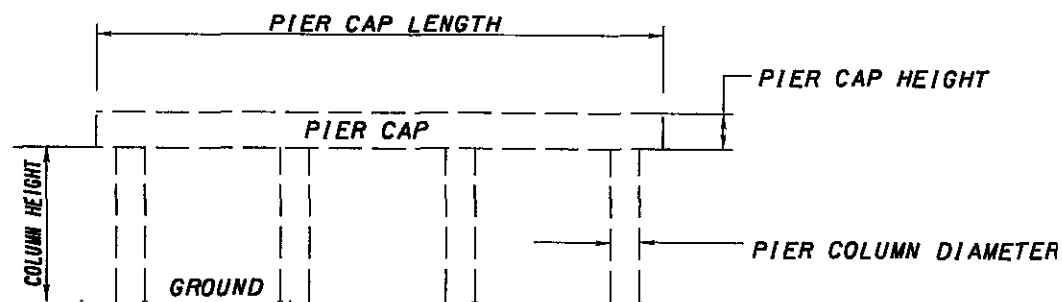
20  
22



TYPICAL PLAN VIEW



ELEVATION VIEW  
TYPE 1



ELEVATION VIEW  
TYPE 2

NOTES:

SEAL ALL PIER COLUMNS AND BOTTOM AND SIDES OF ALL PIER CAPS

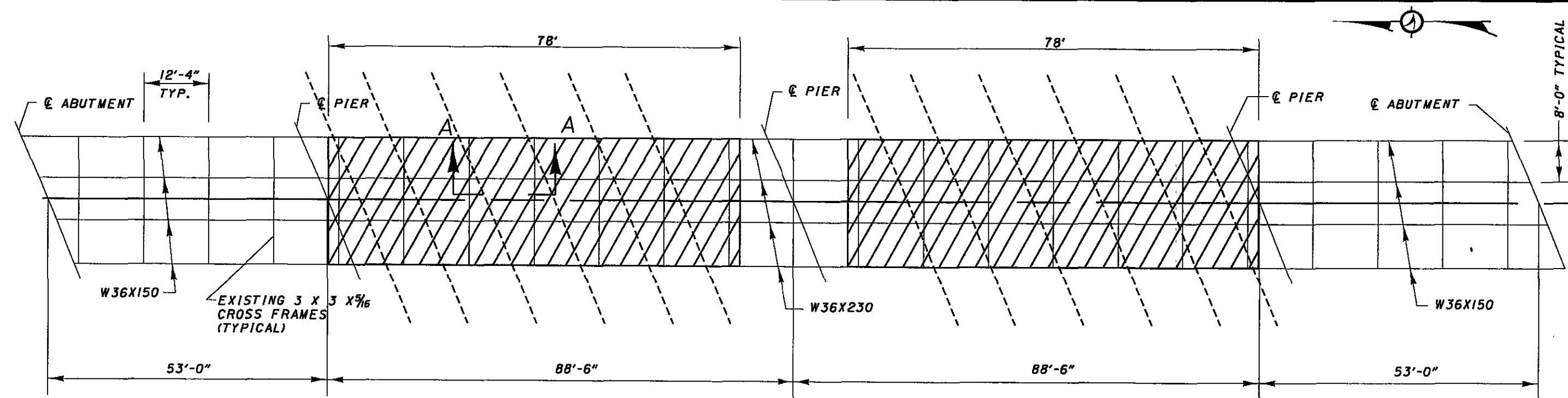
PIER CAPS ARE 3' WIDE

TABLE FOR SEALING OF CONCRETE SURFACES (PIER)

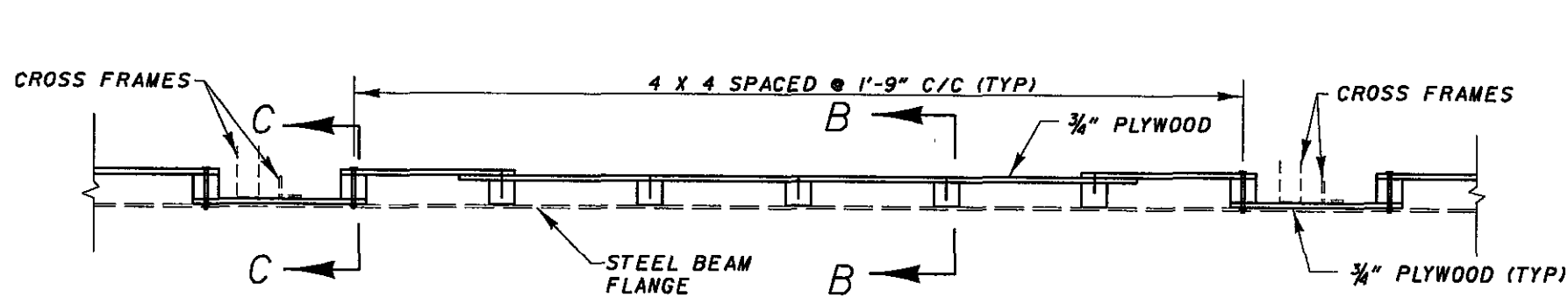
BRIDGE NO.	TYPE	NO. OF PIERS	PIER COLUMN AVERAGE HEIGHT	PIER COLUMN DIAMETER	PIER CAP HEIGHT	PIER CAP LENGTH	* SURFACE AREA TO BE SEALED (SQ. YD.)
RIC-71-1168 UNDER T-308	1	3	12'-7"±	5@4' 4@3'	5'	29'-6"	268
RIC-71-1352 UNDER C-300	1	3	15'-1"±	6@4' 3@3'	5'	33'-0"	316
WAY-71-0222 UNDER T-40	1	3	12'-7"±	9@3'	3'	27'-4"	200
WAY-71-0368 UNDER C-102	2	3	12'-8"±	1@4' 11@3'	3'	42'-3"	288
WAY-71-0508 UNDER C-36	2	3	13'-5"±	12@3'	3'-6"	41'-0"	303
WAY-71-0569 UNDER T-178	1	3	13'-9"±	1@4' 8@3'	4'	34'-4"	262

\* QUANTITY CARRIED TO GENERAL SUMMARY SHEET

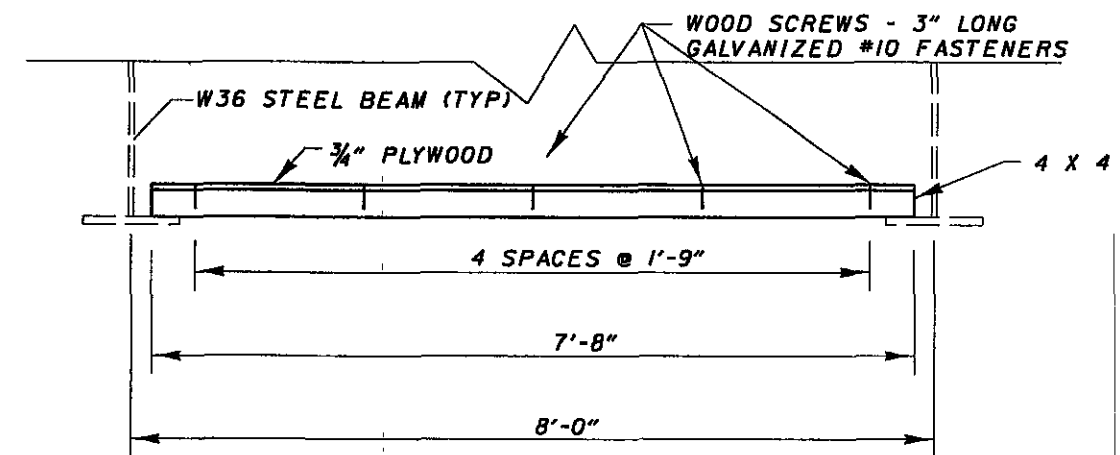
DESIGN FILE: I:\projects\18951\Struct\subdeck.dgn  
WORKSTATION: dmollens  
DATE: 2/13/2006



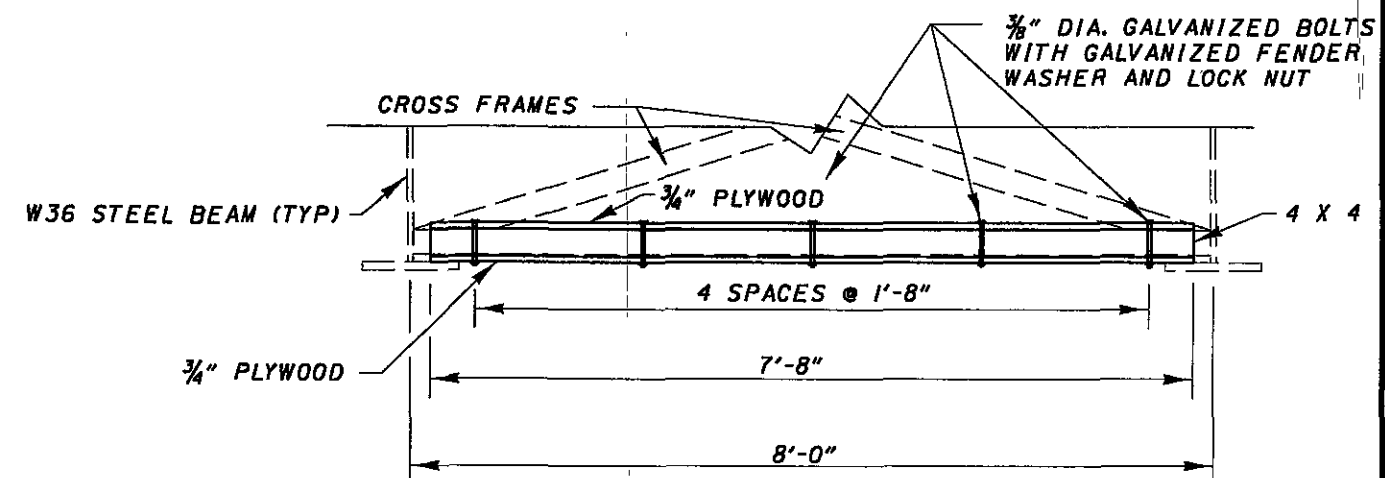
PLAN VIEW



SECTION A-A



SECTION B-B



SECTION C-C

ITEM	QUANTITY	UNIT	DESCRIPTION
SPECIAL	3588	SQ FT	STRUCTURE MISC: TIMBER SUBDECKING

QUANTITY CARRIED TO GENERAL SUMMARY

DESIGN AGENCY  
DISTRICT THREE  
OFFICE OF PRODUCTION

DATE  
1/06  
REVIEWED  
CAL  
STRUCTURE FILE NUMBER  
0303631  
DRAWN  
DCM  
CHECKED  
DCM  
REVISOR  
DJV

TIMBER SUBDECKING DETAILS  
ASD-71-1067

D03-BP-FY2006

1/1

22  
22